

PUBLIC  
RIGHT – OF - WAY  
ACCESSIBILITY  
TRANSITION PLAN



December 2023

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## Introduction

The City of Sandwich has an expansive pedestrian system that links neighborhoods, recreational resources, government facilities, retail centers, and business establishments. Pedestrian accessibility provides benefits not only for people with disabilities, but for the community at large. In an effort to continuously improve the pedestrian system, this document was prepared in accordance with the Americans with Disabilities Act to serve as the Public Right-of-Way Accessibility Transition Plan for the City of Sandwich. This document includes the results of a self-evaluation that addresses accessibility barriers to City programs, activities, and services.

## Federal Requirements

The federal legislation known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990 and later amended effective January 1, 2009, provides comprehensive civil rights protections to qualified persons with disabilities from discrimination on the basis of disability. Title II of the ADA prohibits discrimination in all services, programs, and activities provided to persons with disabilities by State and local governments, including transportation. A self-evaluation is also required by Section 504 of the Rehabilitation Act for all entities receiving federal financial assistance, including federal highway aid for transportation projects. The process of self-evaluation for accessibility provides a framework for communities to develop a Transition Plan that identifies barriers, prioritizes actions to address barriers and then sets forth a schedule to implement those actions.

Under Title II of the ADA the City is required to:

- Designate officials responsible for implementation;
- Provide public notice and opportunity for public participation;
- Develop a grievance procedure;
- Adopt design standards and guidelines;
- Prepare a self-evaluation;
- Schedule and budget for improvements to achieve compliance; and
- Monitor progress

This Transition Plan incorporates these elements and provides a method for the City of Sandwich to schedule and implement required improvements as it pertains to existing pedestrian sidewalks ramps and pathways within the public right-of-way.

## Self-Evaluation

Under Title II of the Americans with Disabilities Act, the City of Sandwich is required to conduct a self-evaluation. To date, the City of Sandwich has conducted a self-evaluation of sidewalk ramps within the public right-of-way. The inventory has been developed using studies of existing aerial photography, street-level imagery, and thorough field survey. Some barriers to the pedestrian access route created by non-compliant curb ramps are narrow sidewalks, buried sidewalk ramps, excessive slope of landings, cross slope, or running slope; damaged, missing, misaligned, improperly sized detectable warning tiles; and not fully depressed or excessively sloped curb & gutter.

## Inventory Methodology of Assets in Public Right-of-Way

A methodology was developed to field inspect existing sidewalk curb ramps which are non-compliant with the requirements of the Americans with Disabilities Act. To begin, all sidewalk ramp locations within the City of Sandwich right-of-way were identified and labeled with a point and given an identification number using available aerial photography.

A preliminary evaluation of sidewalk ramps was performed based on aerial photography and street-level imagery to view each sidewalk ramp. The preliminary investigation evaluated the following five (5) criteria for curb ramps.

- Is there a depressed curb opening with the ramp?
- Does the sidewalk ramp have a compliant color contrasting detectable warning?
- Does the sidewalk ramp have a clear landing at the top of the ramp?
- Is the sidewalk ramp overgrown?
- Is there a receiving ramp on the other side of the street?

If, based on the preliminary review, the sidewalk ramp did not meet the criteria listed above, the ramp was deemed “Non-Compliant”. If the sidewalk ramp did meet the criteria listed above, the ramp was inspected in the field to determine compliance.

The field evaluation was performed by engineering staff using a calibrated smart level to measure the critical dimensions and slopes of the sidewalk ramps. The sidewalk ramp identification number, compliance status, and ramp type (curb ramp, curb less ramp, depressed corner, curb less depressed corner, bike path, ramp to nowhere) were all recorded in an ArcGIS map and recorded in the attached spreadsheet as Exhibit C.

The collected data was evaluated against existing ADA requirements to determine the sidewalk ramp compliance. Pictures of compliant and non-compliant sidewalk ramps are shown in Exhibit A. An aerial map overlaid with current sidewalk ramp compliance status is included as Exhibit B. A spreadsheet of data collected in the field is included as Exhibit C.

## Identification of Barriers

Sidewalk ramps were evaluated according to the maximum allowable slopes as shown in the IDOT Standard Details which are included as Exhibit D. The detail also identifies the length of the curb and gutter transition and curb head tapers. These Standard Details and/or the IDOT Standard Specifications for Road and Bridge Construction specify the minimum dimensions, maximum slopes, materials to be used in the construction of compliant sidewalk ramps. Any sidewalk ramp which does not meet these requirements may create a barrier to pedestrian access.

Detectable warnings must come with a Manufacturer certification stating that the product is fully compliant with accessibility standards, Manufacturer's 5-year warranty, and Manufacturer specifications stating the required materials equipment and installation procedures. The product shall be colored throughout the entire thickness and the alignment of the truncated domes must match the alignment shown in the IDOT Standard Details.

Additionally, the cross slope of sidewalk ramps cannot exceed 2.0%, and the longitudinal slope of sidewalk ramps cannot exceed 8.3%. The longitudinal slope of blended transitions cannot exceed 5.0%. The slope of sidewalk ramp landings cannot exceed 2.0% in any direction.

At locations where sidewalk curb ramps meet curb and gutter, the curb and gutter must be depressed according to the IDOT Standard Detail 606001-07. The cross slope of the curb and gutter cannot exceed 2.0%.

The collected data was entered into a spreadsheet which was used to further determine if the ramps were "Compliant" or "Non-Compliant." Based on the evaluation of the data, the color of the points developed from the aerial were changed so that they can be easily identified in the aerial exhibit. Sidewalk ramps that are compliant appear in the exhibit as green and ramps that are non-compliant appear as red.

## Transition Plan

A transition plan is required by Section 504 of the Rehabilitation Act for all entities receiving federal financial assistance, including federal highway aid for transportation projects, the City of Sandwich is therefore required to develop a public right-of-way accessibility transition plan. This plan will serve as a guide for the City to detail the processes by which accessibility deficiencies will be addressed.

The City is required to designate at least one responsible employee to coordinate ADA compliance for public rights-of-way. This position must be familiar with the City's operation, be properly trained in ADA public right-of-way requirements and be able to effectively communicate with government agencies, advocacy groups and the public. For the duration of the self-evaluation and transition plan schedule, the City has designated the ADA coordinator as the officials responsible to oversee implementation of the transition plan. Any comments, additions or suggestions about this plan may be directed to the ADA coordinator as follows:

The name and contact information for the ADA Coordinator for City of Sandwich is:

**Nick Grube**  
**Superintendent of Streets & Property**  
**207 E. Railroad Street**  
**Sandwich, IL 60548**  
**815-786-8552**  
[ngrube@sandwich.il.us](mailto:ngrube@sandwich.il.us)

## Public Notice

A copy of the Self-Evaluation and Transition Plan will be made available on the City's website at [www.sandwich.il.us](http://www.sandwich.il.us) and by request to the ADA Coordinator or the City Clerk's office. Any resident may address their concerns or comments to the ADA Coordinator listed in this document.

## Grievance Procedure

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 ("ADA"). It may be used by anyone who wishes to file a complaint alleging discrimination based on disability in the provision of services, activities, programs, or benefits by the City of Sandwich. The City's Personnel Policy governs employment-related complaints of disability discrimination.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or his/her designee as soon as possible but no later than 60 calendar days after the alleged violation to the ADA Coordinator listed above. Within 15 calendar days after receipt of the complaint, the ADA Coordinator will meet with the complainant to discuss the complaint and the possible resolutions. Within 15 calendar days of the meeting, the ADA Coordinator will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The response will explain the position of the City of Sandwich and offer options for substantive resolution of the complaint.

If the response by the ADA Coordinator does not satisfactorily resolve the issue, the complainant and/or his/her designee may appeal the decision within 15 calendar days after receipt of the response to the City Administrator.

Within 15 calendar days after receipt of the appeal, the City Administrator will meet with the complainant to discuss the complaint and possible resolutions. Within 15 calendar days after the meeting, the City Administrator or his/her designee will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by the ADA Coordinator, appeals to the City Administrator, and responses from these two offices will be retained by the City of Sandwich for at least three years.

## **Design Standards**

The Department of Justice's revised regulations for Titles II and III of the Americans with Disabilities Act of 1990 (ADA) were published in the Federal Register on September 15, 2010. These regulations adopted revised, enforceable accessibility standards called the 2010 ADA Standards for Accessible Design, "2010 Standards.". On March 15, 2012, compliance with the 2010 Standards was required for new construction and alterations under Titles II and III.

Local Public Agencies may adopt Section 41-6 of the Illinois Department of Transportation Bureau of Local Roads and Streets Manual as a design standard. Section 41-6 is included in this report as Exhibit E.

The City of Sandwich is required to adopt technical guidelines that comply with ADA Standards. Local Public Agencies may adopt Section 41-6 of the Illinois Department of Transportation Bureau of Local Roads and Streets Manual as a design standard. Section 41-6 is included in this report as Exhibit E. In addition to Section 41-6 of the Bureau of Local Roads and Streets Manual the City may adopt any available Standards available from IDOT. The City of Sandwich utilizes all of these standards listed above to ensure ADA compliance under Titles II and III.

## **Self-Evaluation**

The Self-Evaluation shall be included in the Transition Plan. In the case of the City of Sandwich, the Self-evaluation is underway and will continue to be updated as more facilities are evaluated for compliance.

## Schedule and Budget for Improvements

The City of Sandwich will make reasonable efforts to improve the accessibility of pedestrian facilities in the public right-of-way through its annual street resurfacing program and City wide sidewalk replacement program. Federal law requires Local Public Agencies to update non-compliant curb ramps to the maximum extent feasible when the ramps are included within the limits of a street resurfacing project. Therefore, the City is scheduled to update non-compliant curb ramps as streets within the City are resurfaced.

As funds allow, special projects with funding specifically for ADA compliance or pedestrian improvements can be considered. The City will consider the severity of non-compliance, citizen complaints regarding inaccessible locations, poor levels of pedestrian service, population density and the cost when considering an accelerated schedule for replacement.

The anticipated replacement costs of non-compliant ramps which have been field inspected to date is included as Exhibit F. The quantities that were calculated are based on average replacement quantity for different ramp types based on past sidewalk repairs. The costs that are shown are based upon an average of the bid tabulations from recent sidewalk ramp repairs. A 20% adjustment factor was added to the cost and quantities to account for additional contingency work and engineering costs.

## Monitoring Progress

In order to be effective, the Public Right-of-Way Accessibility Transition Plan needs to be utilized in planning and funding decisions. Therefore, it is important to update the plan regulatory to reflect changes in real-world conditions and to address any possible new areas of non-compliance. Regular updates will also result in monitoring compliance and the effectiveness of priorities set in the plan itself.

The City of Sandwich intends to make regular updates to this plan as non-compliant ramps are replaced and as additional non-compliant features are evaluated. The City is committed to acting on the guidelines set forth in this document and also to actively revising and amending the plan as progress is made.



# EXHIBIT A

## EXAMPLES OF COMPLIANT AND NON-COMPLIANT RAMPS



The above are examples of depressed corners that don't meet the current ADA standards. The crossings don't have any Detectable Warnings and/or the existing cross slope does not meet the current ADA standards.



The above are examples of curb ramps that don't meet the current ADA standards. The crossings don't have any Detectable Warnings. Also, the existing cross slope does not meet the current ADA standards.

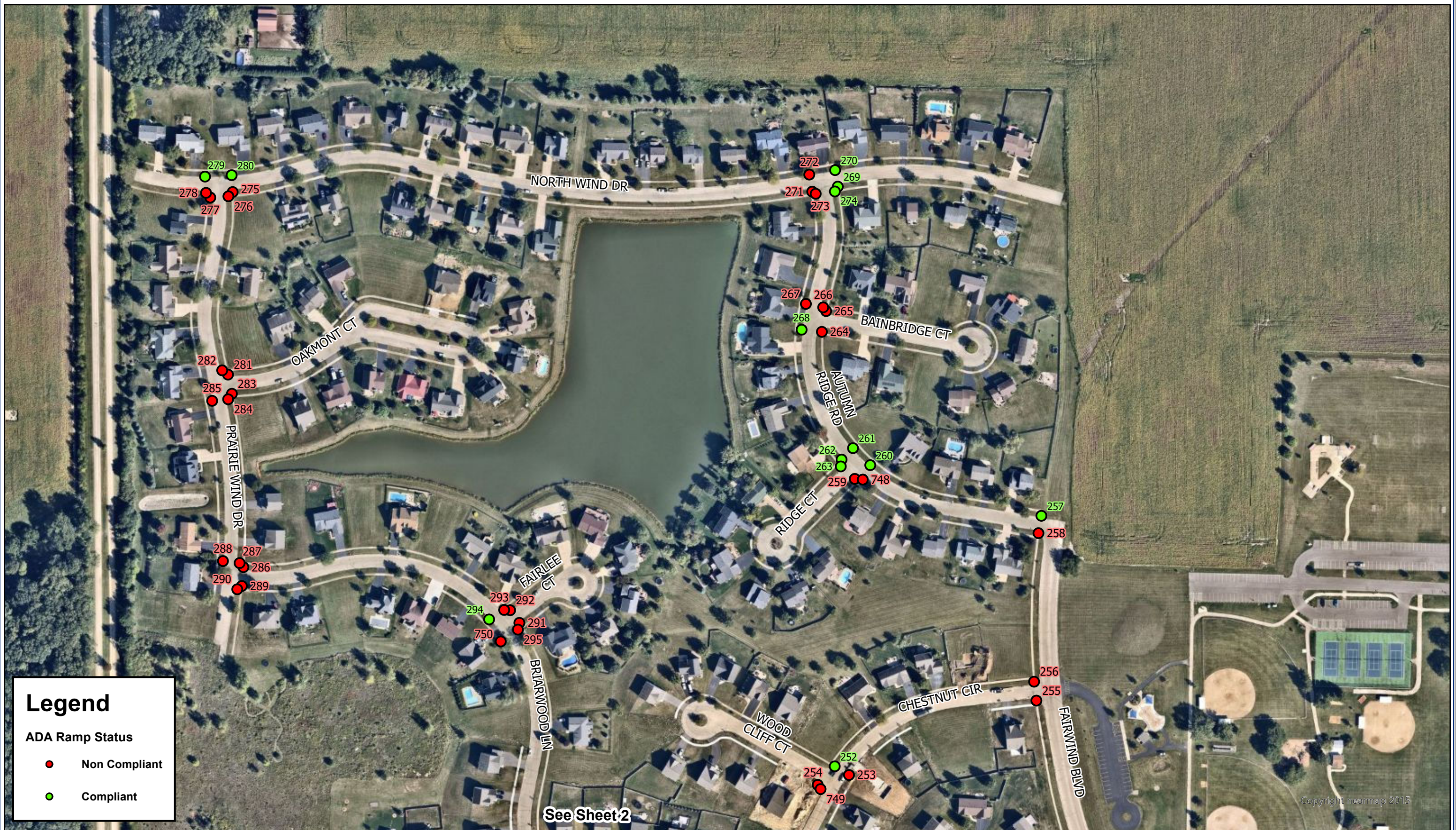


The above curb ramps and depressed corners consist of detectable warnings and geometric slopes that are “compliant” with the current ADA standards. All future construction will meet the current ADA standards.

# EXHIBIT B

## ATLAS MAP OF RAMP COMPLIANCE





### Legend

#### ADA Ramp Status

- Non Compliant
- Compliant

See Sheet 2

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 www.eeiweb.com



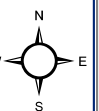
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**City of Sandwich  
 Sidewalk Atlas: Sheet 1**





**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

See Sheet 5

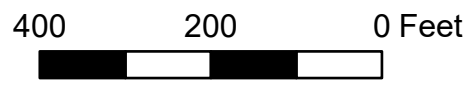
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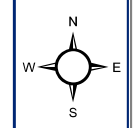
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**City of Sandwich  
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**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

See Sheet 5

See Sheet 6

See Sheet 4

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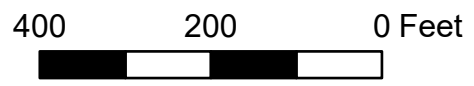
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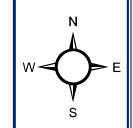
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**City of Sandwich  
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**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

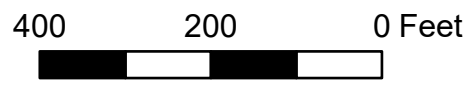
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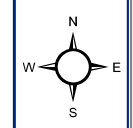
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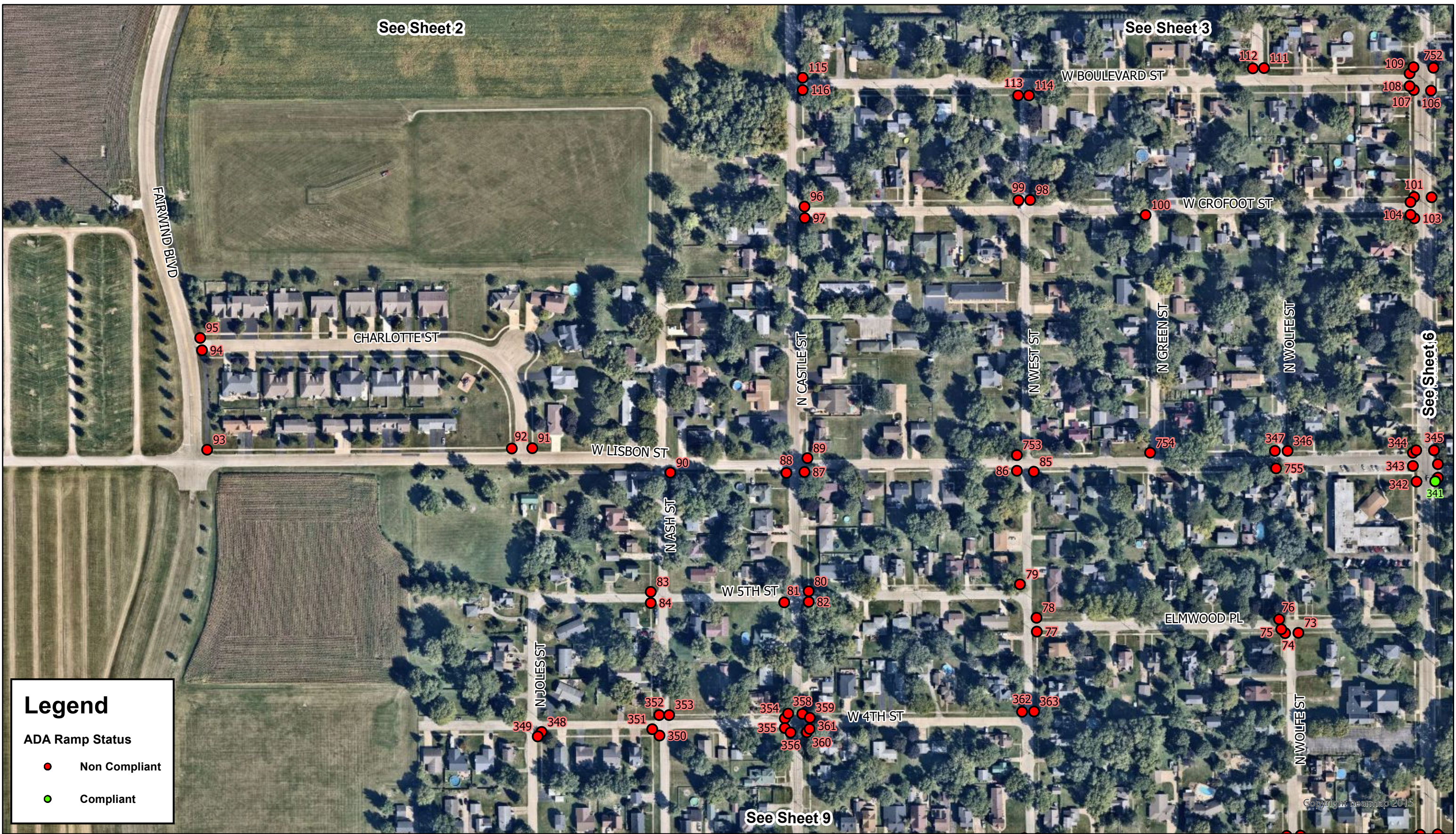
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### Legend

**ADA Ramp Status**

- Non Compliant
- Compliant

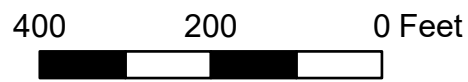
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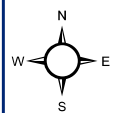
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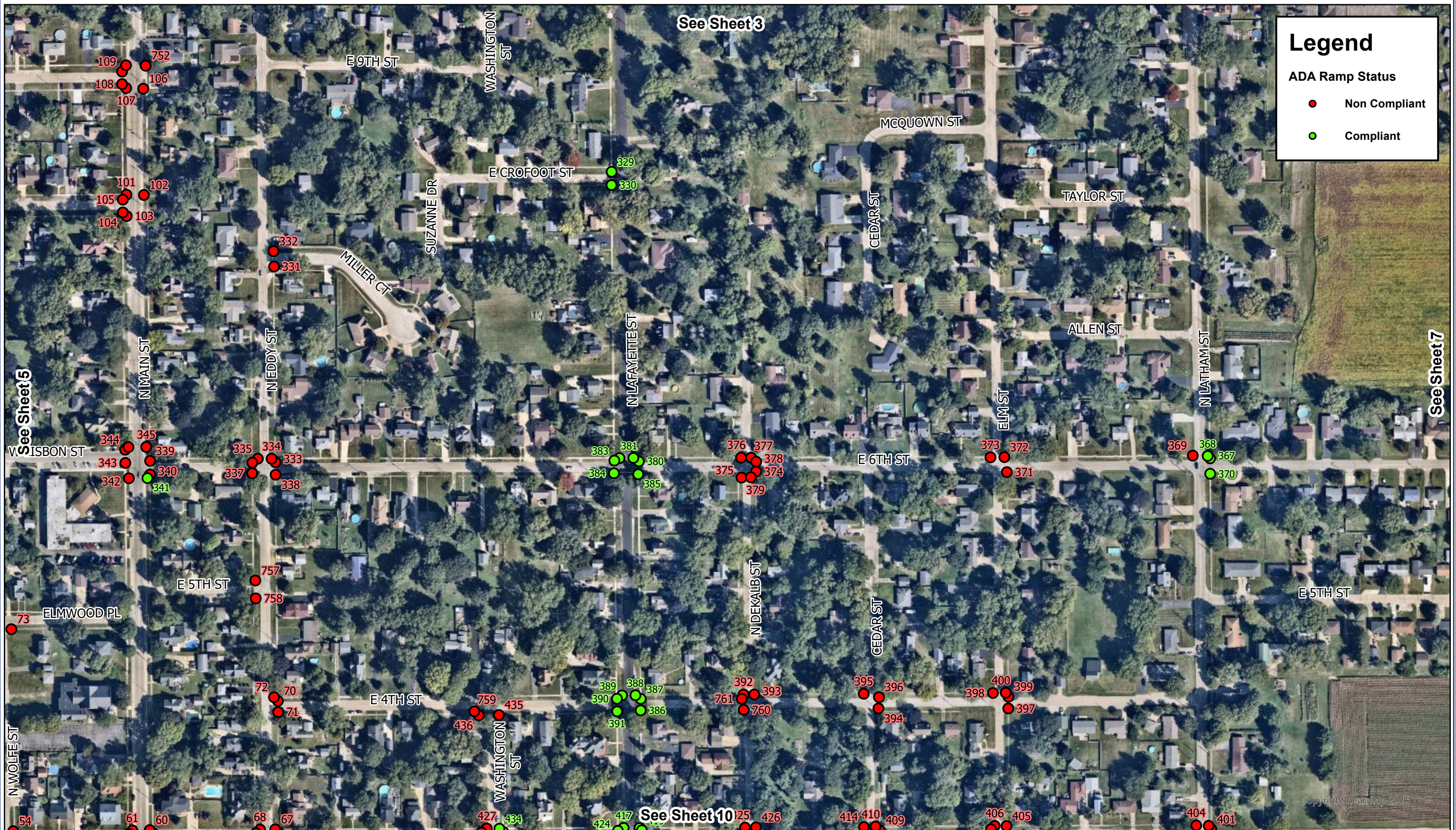
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**City of Sandwich  
 Sidewalk Atlas: Sheet 5**





### Legend

ADA Ramp Status

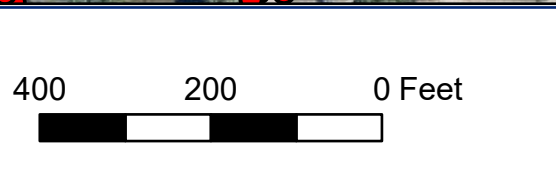
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- Compliant

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See Sheet 6

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- Non Compliant
- Compliant

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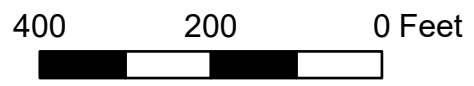
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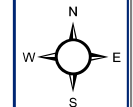
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See Sheet 9

**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

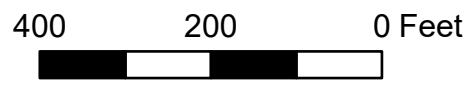
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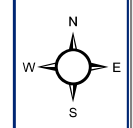
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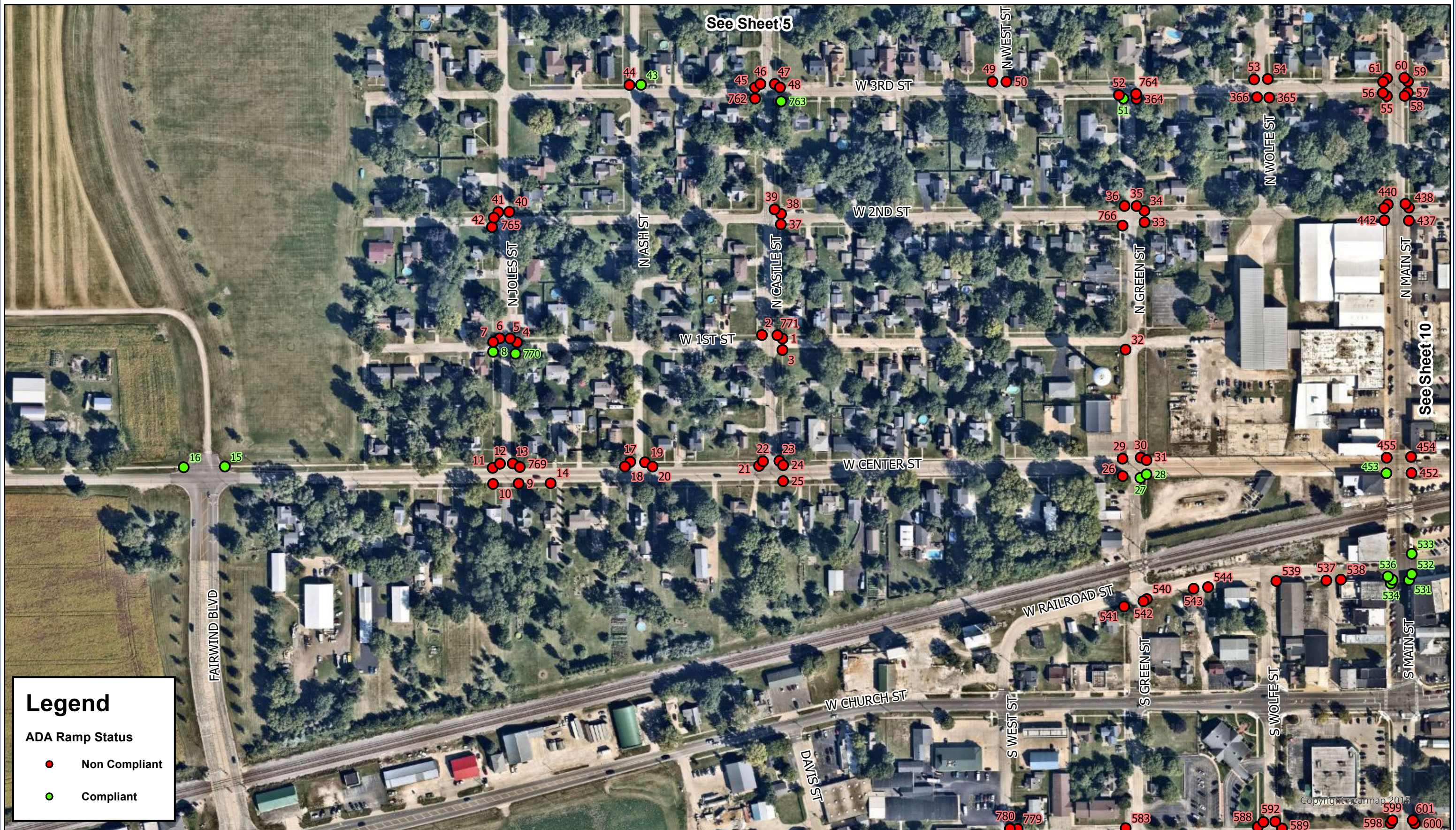
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### Legend

- ADA Ramp Status
- Non Compliant
  - Compliant

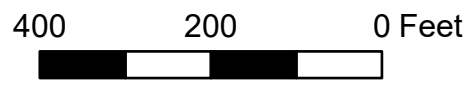
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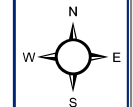
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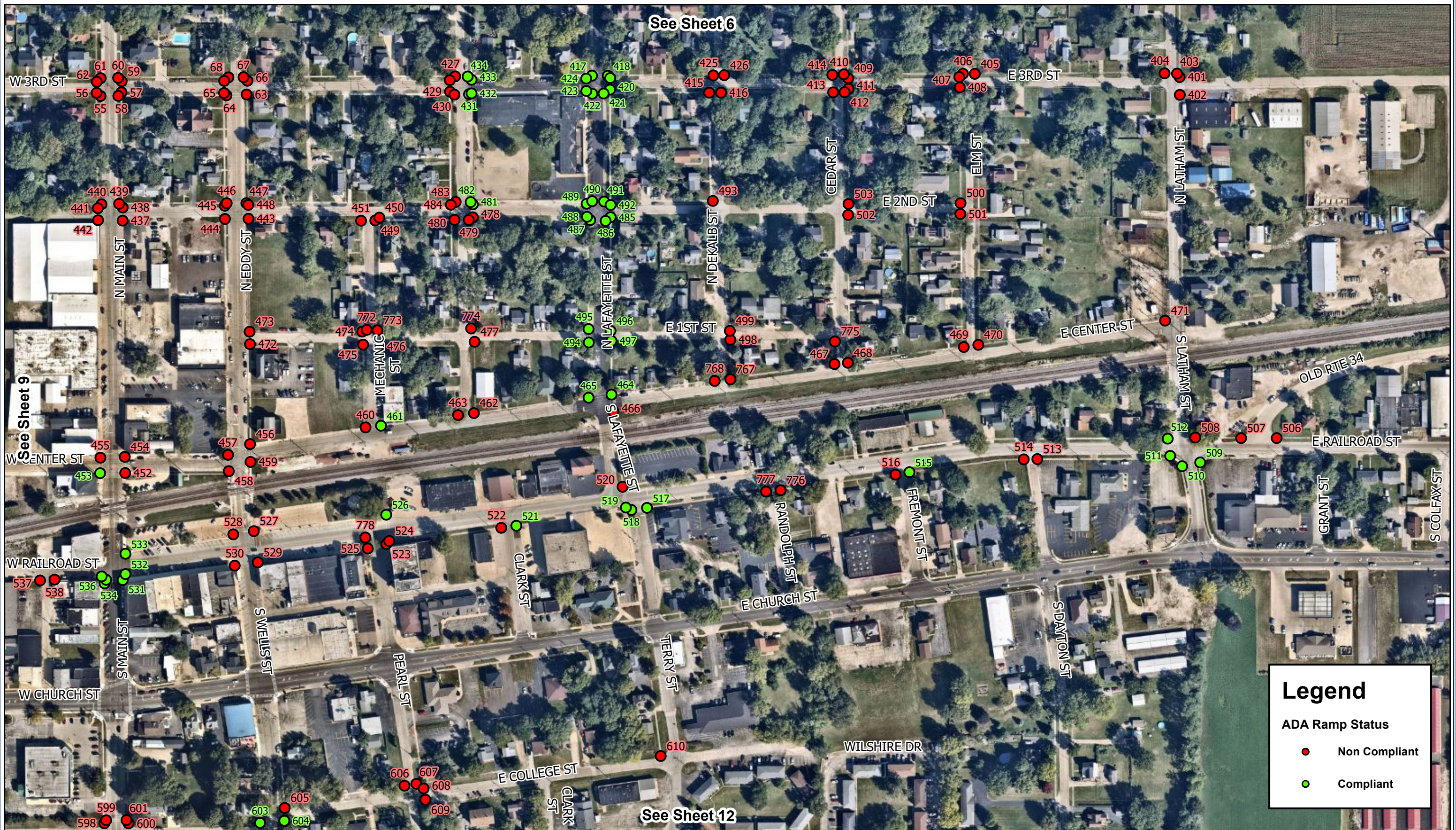
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### City of Sandwich Sidewalk Atlas: Sheet 9





**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

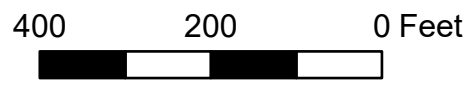
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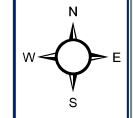
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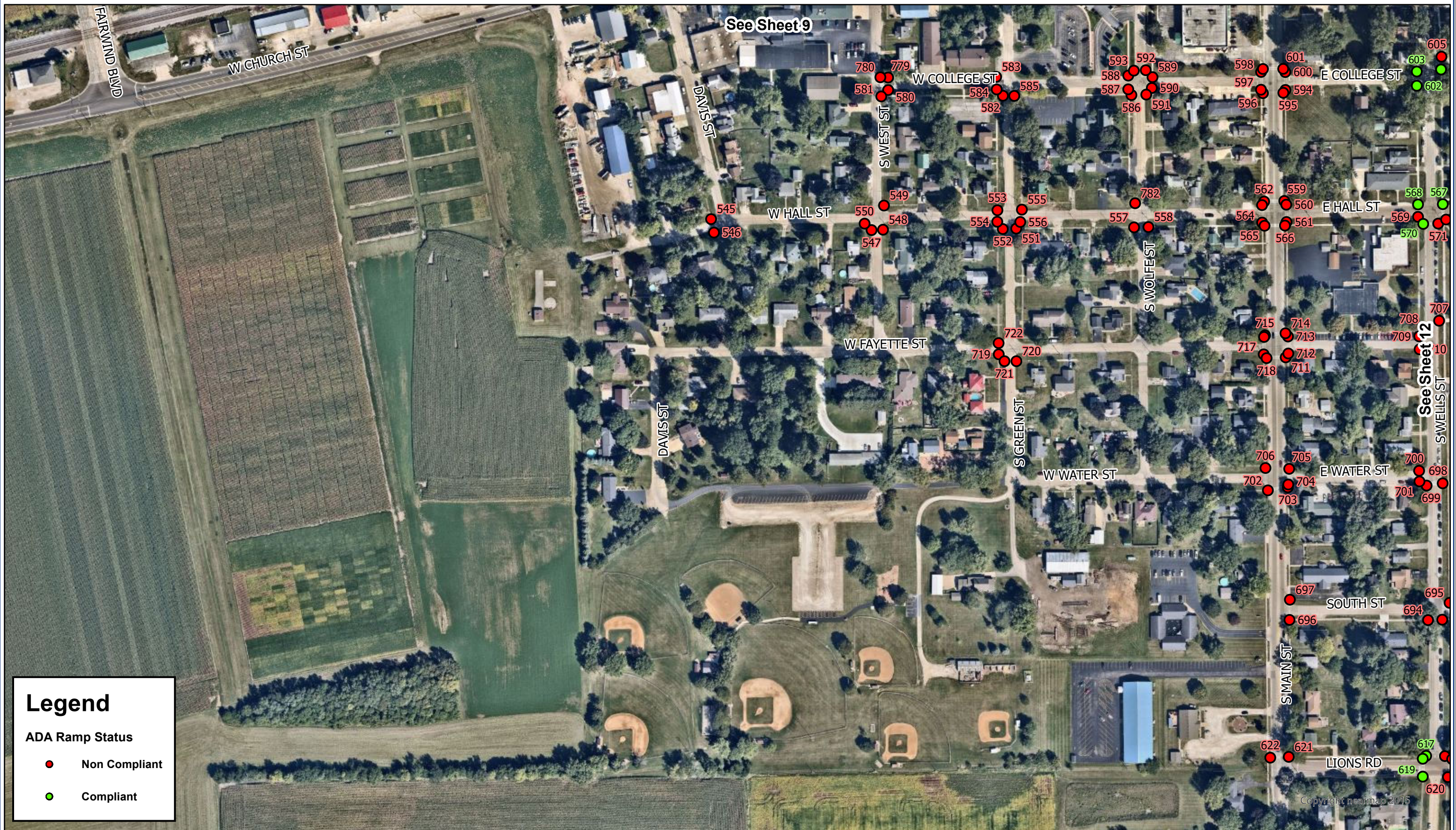
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**City of Sandwich  
 Sidewalk Atlas: Sheet 10**





**Legend**

ADA Ramp Status

- Non Compliant
- Compliant

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### Legend

ADA Ramp Status

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- Compliant

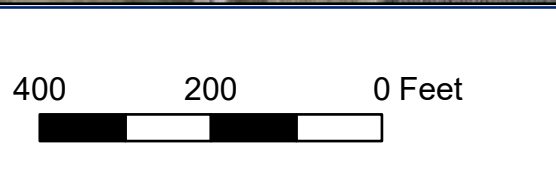
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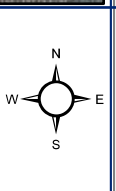
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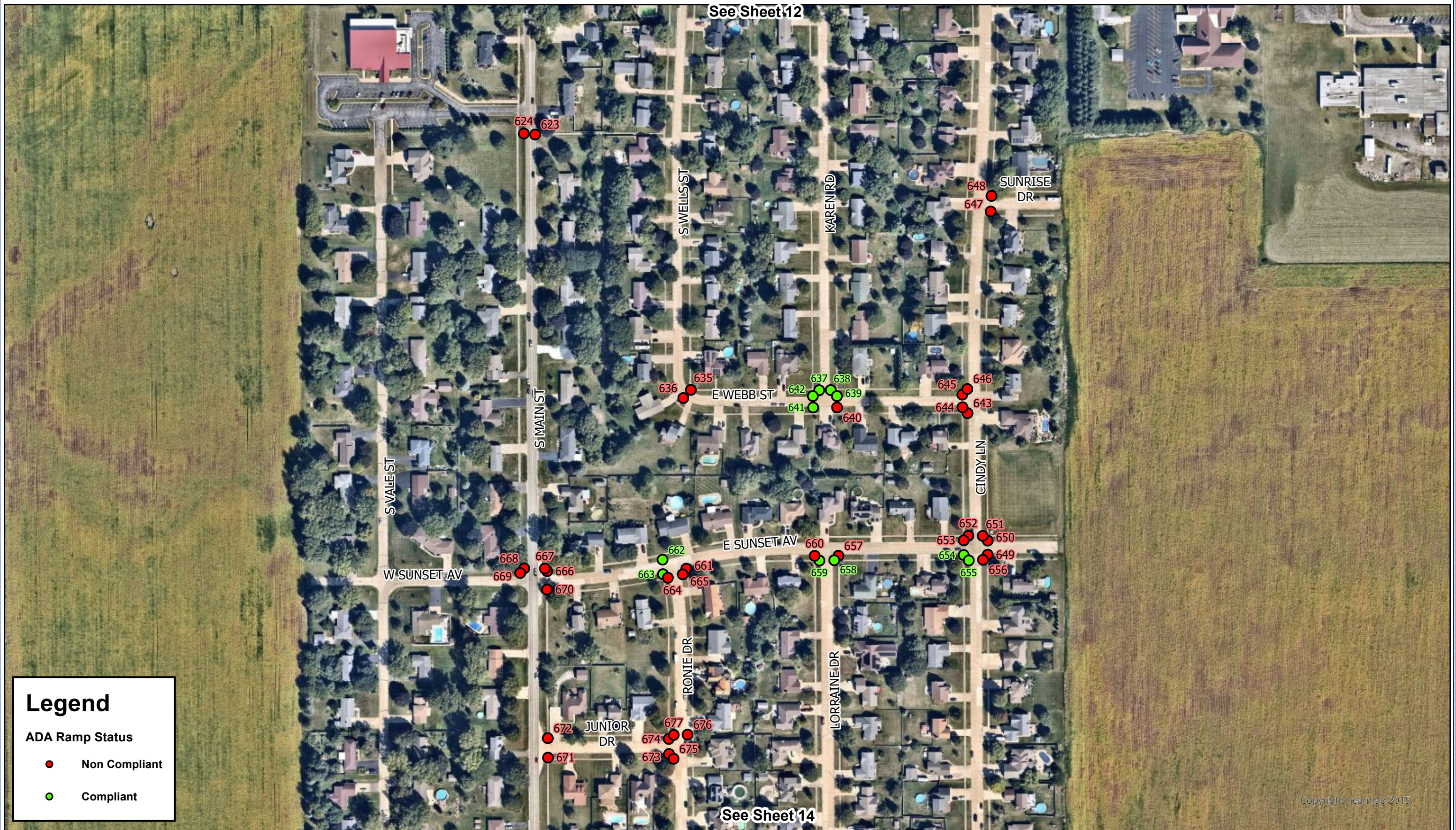
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### Legend

- ADA Ramp Status
- Non Compliant
  - Compliant

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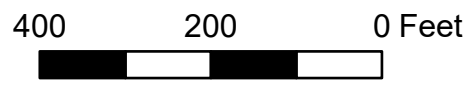
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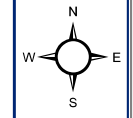
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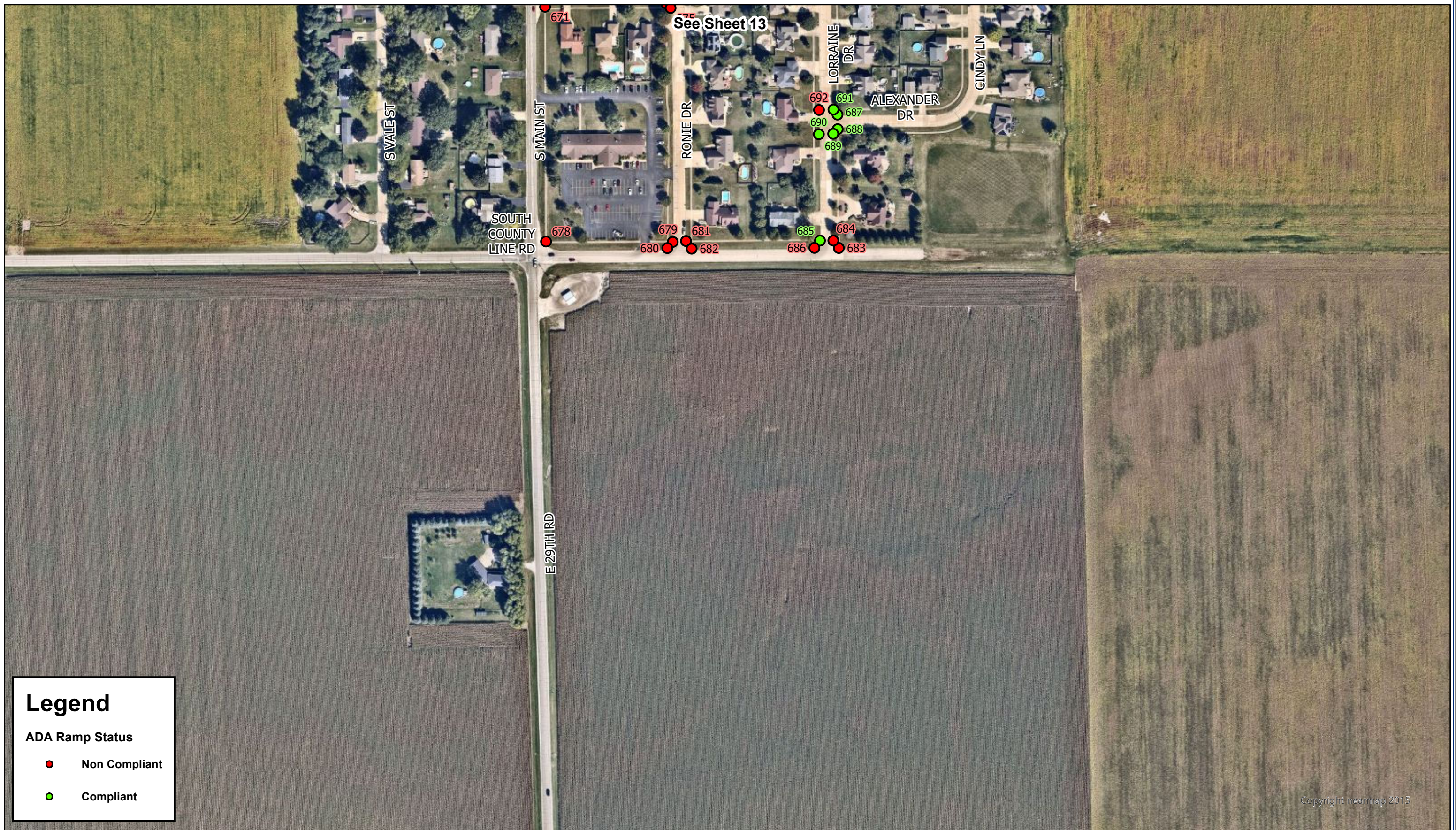
NO.	DATE	REVISIONS

DATE:	DECEMBER 2023
PROJECT NO.:	SA2321
BY:	MJT
PATH:	H:\GIS\PUBLIC\Sandwich\2023\
FILE:	SA2321_Sidewalk_Sheet 13



**City of Sandwich  
 Sidewalk Atlas: Sheet 13**





### Legend

- ADA Ramp Status
- Non Compliant
  - Compliant

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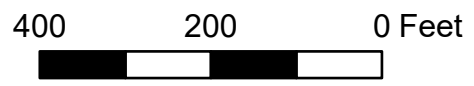
**Engineering Enterprises, Inc.**  
 52 Wheeler Road  
 Sugar Grove, Illinois 60554  
 (630) 466-6700  
 www.eeiweb.com



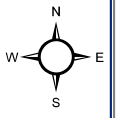
**City of Sandwich**  
 144 E. Railroad St.  
 Sandwich, IL 60548  
 815-786-9321

NO.	DATE	REVISIONS

DATE:	DECEMBER 2023
PROJECT NO.:	SA2321
BY:	MJT
PATH:	H:\GIS\PUBLIC\Sandwich\2023\
FILE:	SA2321_Sidewalk_Sheet 14



**City of Sandwich  
 Sidewalk Atlas: Sheet 14**





**Legend**

- ADA Ramp Status
- Non Compliant
  - Compliant

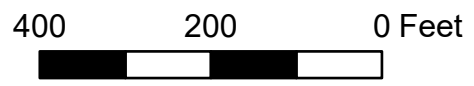
**Engineering Enterprises, Inc.**  
 52 Wheeler Road  
 Sugar Grove, Illinois 60554  
 (630) 466-6700  
 www.eeiweb.com



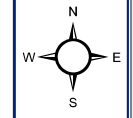
**City of Sandwich**  
 144 E. Railroad St.  
 Sandwich, IL 60548  
 815-786-9321

NO.	DATE	REVISIONS

DATE: DECEMBER 2023  
 PROJECT NO.: SA2321  
 BY: MJT  
 PATH: H:\GIS\PUBLIC\Sandwich\2023\  
 FILE: SA2321\_Sidewalk\_Sheet 15



**City of Sandwich  
 Sidewalk Atlas: Sheet 15**



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# EXHIBIT C

## COMPILED FIELD DATA

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
1		X		X				
2		X		X				
3		X		X				
4		X		X				
5		X		X				
6		X		X				
7		X		X				
8	X			X				
9		X	X					
10		X	X					
11		X	X					
12		X	X					
13		X	X					
14		X						X
15	X		X					
16	X		X					
17		X	X					
18		X	X					X
19		X	X					
20		X	X					X
21		X	X					X
22		X	X					
23		X	X					
24		X	X					
25		X	X					
26		X			X			
27	X		X					
28	X		X					
29		X			X			
30		X	X					
31		X	X					
32		X		X				X
33		X		X				
34		X		X				
35		X		X				
36		X		X				
37		X		X				
38		X		X				
39		X		X				X
40		X		X				
41		X		X				
42		X		X				
43	X			X				
44		X		X				
45		X		X				
46		X		X				
47		X		X				
48		X		X				
49		X		X				
50		X		X				
51	X			X				
52		X		X				X
53		X		X				
54		X		X				
55		X	X					

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
56		X	X					
57		X	X					
58		X	X					
59		X	X					
60		X	X					
61		X	X					
62		X	X					
63		X			X			
64		X	X					
65		X	X					
66		X	X					
67		X	X					
68		X	X					
69		X	X					
70		X		X				
71		X		X				
72		X		X				X
73		X		X				
74		X		X				
75		X		X				
76		X		X				
77		X		X				
78		X		X				
79		X		X				X
80		X		X				
81		X		X				X
82		X		X				
83		X		X				
84		X		X				
85		X		X				
86		X		X				
87		X		X				
88		X		X				
89		X		X				
90		X		X				X
91		X			X			
92		X			X			
93		X			X			X
94		X	X				X	
95		X	X				X	
96		X		X				
97		X		X				
98		X		X				
99		X		X				
100		X		X				X
101		X	X					
102		X	X					
103		X	X					X
104		X	X					
105		X	X					
106		X	X					
107		X	X					
108		X	X					
109		X	X					
110		X	X					

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
111		X		X				
112		X		X				
113		X		X				
114		X		X				
115		X		X				
116		X		X				
117		X		X				
118		X		X				
119		X		X				
120		X		X				
121		X		X				X
122		X	X					
123		X	X					
124		X	X					
125		X	X					
126		X		X				
127		X				X		
128		X		X				
129		X		X				
130	X			X				
131		X		X				
132		X		X				
133	X			X				
134		X	X					
135		X	X					
136		X	X					
137		X	X					X
138		X	X					
139		X	X					
140		X	X					X
141		X	X					
142		X	X					
143		X	X					
144		X	X					
145		X	X					X
146		X	X					
147		X	X					
148		X	X					
149		X	X					
150		X	X					
151		X	X					
152		X	X					
153		X	X					
154		X	X					X
155		X	X					
156		X	X					
157		X	X					X
158	X		X					
159	X		X					
160		X	X					
161		X	X					
162		X	X					
163		X	X					
164		X	X					X
165		X	X					



**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
166		X	X					
167		X	X					X
168		X	X					X
169		X	X					
170		X	X					
171		X	X					X
172		X	X					
173		X	X					
174		X	X					
175		X	X					
176		X	X					
177		X	X					
178		X	X					
179		X	X					
180		X	X					
181		X	X					
182		X	X					
183		X	X					
184		X	X					
185		X	X					
186		X	X					
187		X	X					
188		X	X					X
189		X	X					
190		X	X					
191		X	X					X
192		X	X					
193		X	X					
194		X	X					
195		X	X					
196		X	X					
197		X	X					
198		X	X					
199		X	X					
200	X		X					
201		X						X
202		X	X					
203		X	X					X
204		X	X					X
205		X	X					
206		X	X					
207		X				X		X
208		X				X		X
209	X			X				
210	X			X				
211		X		X				X
212		X		X				
213		X		X				
214		X	X					
215		X	X					
216		X	X					
217		X			X			X
218	X			X				
219	X			X				
220	X		X					

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
221		X	X					
222		X	X					
223		X	X					
224		X	X					
225		X	X					X
226		X	X					
227		X	X					
228		X	X					
229		X	X					
230		X	X					
231		X	X					
232		X	X					
233		X	X					
234		X	X					
235		X	X					
236		X	X					
237		X	X					
238		X	X					
239		X	X					
240		X	X					
241		X	X					
242		X	X					
243		X	X					
244		X	X					X
245		X	X					X
246		X	X					
247		X	X					
248		X	X					X
249		X	X					X
250		X	X					
251		X	X					
252	X		X					
253		X	X					
254		X	X					X
255		X	X					
256		X	X					
257	X		X					
258		X	X					
259		X	X					
260	X		X					
261	X		X					
262	X		X					
263	X		X					
264		X			X			
265		X	X					
266		X	X					
267		X	X					
268	X		X					
269	X		X					
270	X		X					
271		X	X					
272		X	X					
273		X	X					
274	X		X					
275		X	X					

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
276		X	X					
277		X	X					
278		X	X					
279	X		X					
280	X		X					
281		X	X					
282		X	X					X
283		X	X					
284		X	X					
285		X	X					
286		X	X					
287		X	X					
288		X	X					
289		X	X					
290		X	X					X
291		X	X					
292		X	X					
293		X	X					
294	X		X					
295		X	X					
296		X		X				X
297		X	X					
298		X	X					
299		X	X					
300		X	X					
301		X	X					
302		X	X					
303		X	X					
304		X	X					
305		X	X					
306		X	X					
307		X	X					
308		X	X					
309		X	X					
310		X	X					
311		X	X					
312		X	X					
313		X	X					
314		X	X					
315		X	X					
316		X	X					
317		X	X					
318		X	X					
319		X	X					
320		X	X					
321		X	X					
322		X	X					
323		X	X					
324		X	X					X
325		X	X					
326		X	X					
327		X	X					
328		X		X				X
329	X			X				
330	X			X				

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
331		X	X					
332		X	X					
333		X		X				
334		X		X				
335		X		X				
336		X		X				
337		X		X				
338		X		X				
339		X	X					
340		X	X					
341	X		X					
342		X	X					
343		X	X					
344		X	X					
345		X	X					
346		X		X				
347		X				X		
348		X		X				X
349		X		X				X
350		X		X				X
351		X		X				X
352		X		X				
353		X		X				
354		X		X				
355		X		X				
356		X		X				
357		X		X				
358		X		X				
359		X		X				
360		X		X				
361		X		X				
362		X		X				
363		X		X				
364		X		X				
365		X		X				
366		X		X				
367	X			X				
368	X			X				
369		X		X				
370	X			X				
371		X		X				X
372		X		X				
373		X		X				
374		X		X				
375		X		X				
376		X		X				
377		X		X				
378		X		X				
379		X		X				
380	X			X				
381	X			X				
382	X			X				
383	X			X				
384	X			X				
385	X			X				

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
386	X			X				
387	X			X				
388	X			X				
389	X			X				
390	X			X				
391	X			X				
392		X		X				
393		X		X				
394		X		X				
395		X		X				X
396		X		X				
397		X		X				
398		X		X				
399		X		X				
400		X		X				
401		X		X				
402		X		X				
403		X		X				
404		X		X				
405		X		X				
406		X		X				
407		X		X				
408		X		X				
409		X		X				
410		X		X				
411		X		X				
412		X		X				
413		X		X				
414		X		X				
415		X		X				
416		X		X				
417	X			X				
418	X			X				
419	X			X				
420	X			X				
421	X			X				
422	X			X				
423	X			X				
424	X			X				
425		X		X				
426		X		X				
427		X		X				
428		X		X				
429		X		X				
430		X		X				
431	X			X				
432	X		X					
433	X			X				
434	X			X				
435		X		X				
436		X		X				
437		X			X			
438		X	X					
439		X	X					
440		X	X					

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
441		X	X					
442		X	X					
443		X	X					
444		X	X					
445		X	X					
446		X	X					
447		X	X					
448		X	X					
449		X		X				
450		X		X				X
451		X		X				
452		X			X			
453	X				X			
454		X			X			
455		X			X			
456		X			X			
457		X			X			
458		X	X					
459		X		X				
460		X	X					
461	X		X					
462		X	X					
463		X	X					
464	X					X		
465	X		X					
466		X		X				
467		X		X				
468		X		X				
469		X		X				
470		X		X				
471		X		X				X
472		X	X					
473		X	X					
474		X		X				
475		X		X				
476		X		X				X
477		X		X				
478		X		X				
479		X		X				
480		X			X			
481	X			X				
482	X			X				
483		X		X				
484		X		X				
485	X			X				
486	X			X				
487	X			X				
488	X			X				
489	X			X				
490	X			X				
491	X			X				
492	X			X				
493		X		X				X
494	X			X				
495	X			X				

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
496	X			X				
497	X			X				
498		X		X				
499		X		X				
500		X		X				
501		X		X				
502		X		X				
503		X		X				
504		X	X					
505		X		X				
506		X	X					
507		X	X					
508		X	X					
509	X				X			
510	X		X					
511	X		X					
512	X		X					
513		X	X					
514		X	X					
515	X		X					
516		X	X					
517	X		X					
518	X		X					
519	X		X					
520		X	X					
521	X		X					
522		X	X					
523		X	X					
524		X	X					
525		X	X					
526	X		X					
527		X	X					
528		X	X					
529		X			X			
530		X			X			
531	X		X					
532	X		X					
533	X		X					
534	X		X					
535	X		X					
536	X		X					
537		X	X					
538		X	X					
539		X		X				X
540		X		X				X
541		X		X				
542		X		X				
543		X		X				
544		X		X				
545		X		X				
546		X		X				
547		X		X				
548		X		X				
549		X		X				X
550		X		X				X

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
551		X		X				
552		X		X				
553		X		X				
554		X		X				
555		X		X				
556		X		X				
557		X		X				
558		X		X				
559		X	X					
560		X	X					
561		X	X					
562		X	X					
563		X	X					
564		X	X					
565		X	X					
566		X	X					
567	X			X				
568	X			X				
569		X		X				
570	X			X				
571		X		X				
572		X	X					
573		X		X				
574		X		X				X
575		X		X				X
576		X		X				
577		X		X				
578		X		X				
579		X		X				
580		X		X				
581		X		X				X
582		X		X				
583		X		X				
584		X		X				
585		X		X				
586		X		X				
587		X		X				
588		X		X				
589		X		X				
590		X		X				
591		X		X				
592		X		X				
593		X		X				
594		X	X					
595		X	X					
596		X	X					
597		X	X					
598		X	X					
599		X	X					
600		X	X					
601		X	X					
602	X			X				
603	X			X				
604	X			X				
605		X		X				



**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
606		X		X				
607		X		X				
608		X		X				
609		X		X				
610		X		X				X
611		X	X					
612		X	X					
613		X	X					
614		X	X					
615		X	X					
616		X	X					
617	X		X					
618	X		X					
619	X		X					
620		X	X					
621		X	X					
622		X	X					
623		X		X				
624		X		X				
625		X	X					
626		X	X					
627		X	X					
628		X	X					
629		X	X					
630		X	X					
631		X		X				
632		X		X				
633		X	X					
634		X	X					
635		X	X					
636		X	X					
637	X		X					
638	X		X					
639	X		X					
640		X	X					
641	X		X					
642	X		X					
643		X	X					
644		X	X					
645		X	X					
646		X	X					X
647		X	X					
648		X	X					
649		X	X					
650		X	X					
651		X	X					
652		X	X					
653		X	X					
654	X		X					
655	X		X					
656		X	X					
657		X	X					X
658	X		X					
659	X		X					
660		X	X					X

**EXHIBIT C - COMPILED FIELD DATA**

RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
661		X	X					X
662	X		X					
663	X		X					
664		X	X					
665		X	X					
666		X	X					
667		X	X					
668		X		X				
669		X		X				X
670		X	X					
671		X	X					
672		X	X					
673		X	X					
674		X	X					
675		X	X					X
676		X	X					
677		X	X					
678		X	X					
679		X	X					
680		X	X					X
681		X	X					
682		X	X					X
683		X	X					X
684		X	X					
685	X		X					
686		X	X					X
687	X		X					
688	X		X					
689	X		X					
690	X		X					
691	X		X					
692		X	X					
693		X			X			
694		X		X				
695		X	X					
696		X	X					
697		X	X					
698		X	X					
699		X		X				
700		X		X				
701		X		X				
702		X			X			
703		X	X					
704		X	X					
705		X	X					
706		X			X			
707		X	X					
708		X		X				
709		X		X				
710		X		X				
711		X	X					
712		X	X					
713		X	X					
714		X	X					
715		X	X					

**EXHIBIT C - COMPILED FIELD DATA**

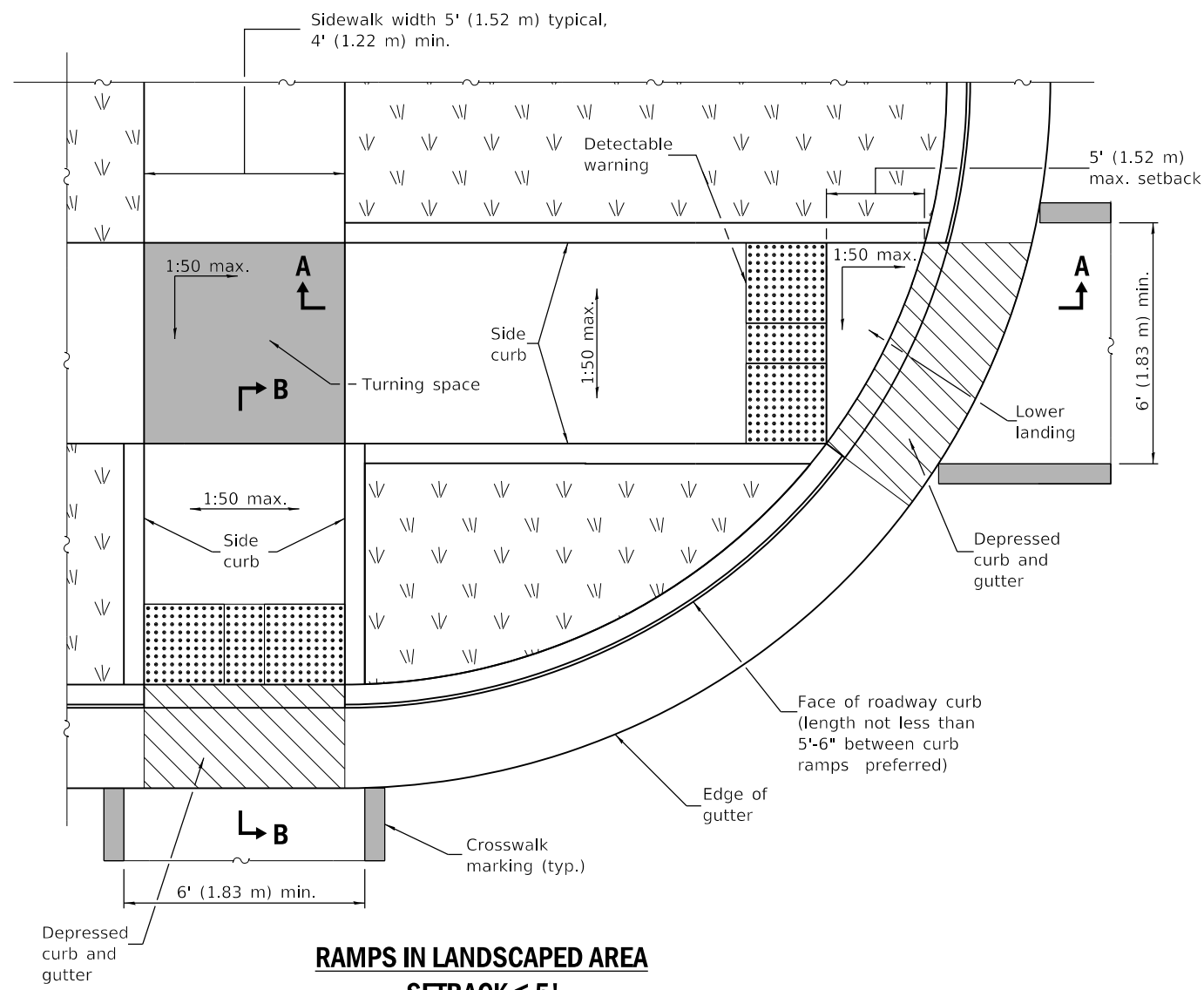
RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
716		X	X					
717		X	X					
718		X	X					
719		X		X				
720		X		X				
721		X		X				
722		X		X				
723		X	X					
724		X	X					
725		X	X					
726		X	X					
727		X	X					
728		X	X					
729		X	X					
730	X		X					
731	X		X					
732		X	X					X
733		X	X					X
734		X	X					
735		X	X					
736		X	X					
737		X	X					
738		X	X					
739	X		X					
740		X	X					X
741	X				X			
742		X	X					X
743		X		X				
744		X		X				
745		X		X				
746		X		X				
747		X	X					
748		X	X					
749		X	X					X
750		X	X					
751		X		X				
752		X	X					
753		X		X				X
754		X		X				X
755		X		X				
756		X	X					
757		X		X				
758		X		X				
759		X		X				X
760		X		X				
761		X		X				
762		X		X				
763	X			X				
764		X		X				X
765		X		X				
766		X		X				X
767		X		X				
768		X		X				
769		X	X					
770	X			X				

**EXHIBIT C - COMPILED FIELD DATA**

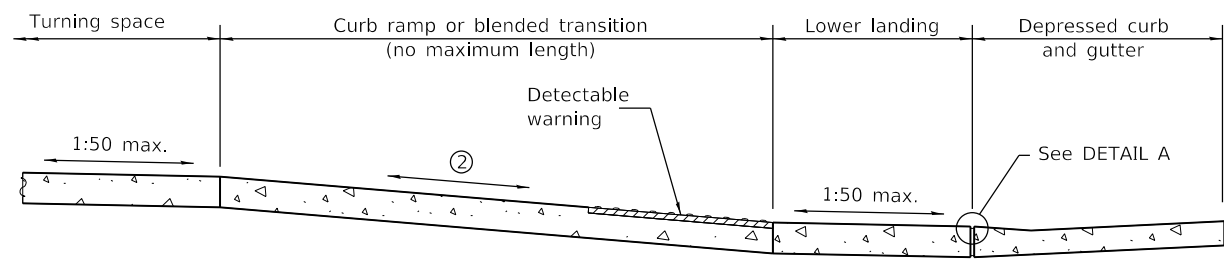
RAMP ID #	RAMP COMPLIANCE		TYPE OF RAMP					
	Compliant	Non-Compliant	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere
771		X		X				
772		X		X				
773		X		X				
774		X		X				
775		X		X				X
776		X	X					
777		X	X					
778		X	X					X
779		X		X				
780		X		X				
781		X		X				
782		X		X				X
783		X		X				
784		X		X				
<b>TOTAL</b>	126	658	429	325	23	5	2	85

# EXHIBIT D

## ILLINOIS DEPARTMENT OF TRANSPORTATION CURB RAMP STANDARD DETAILS

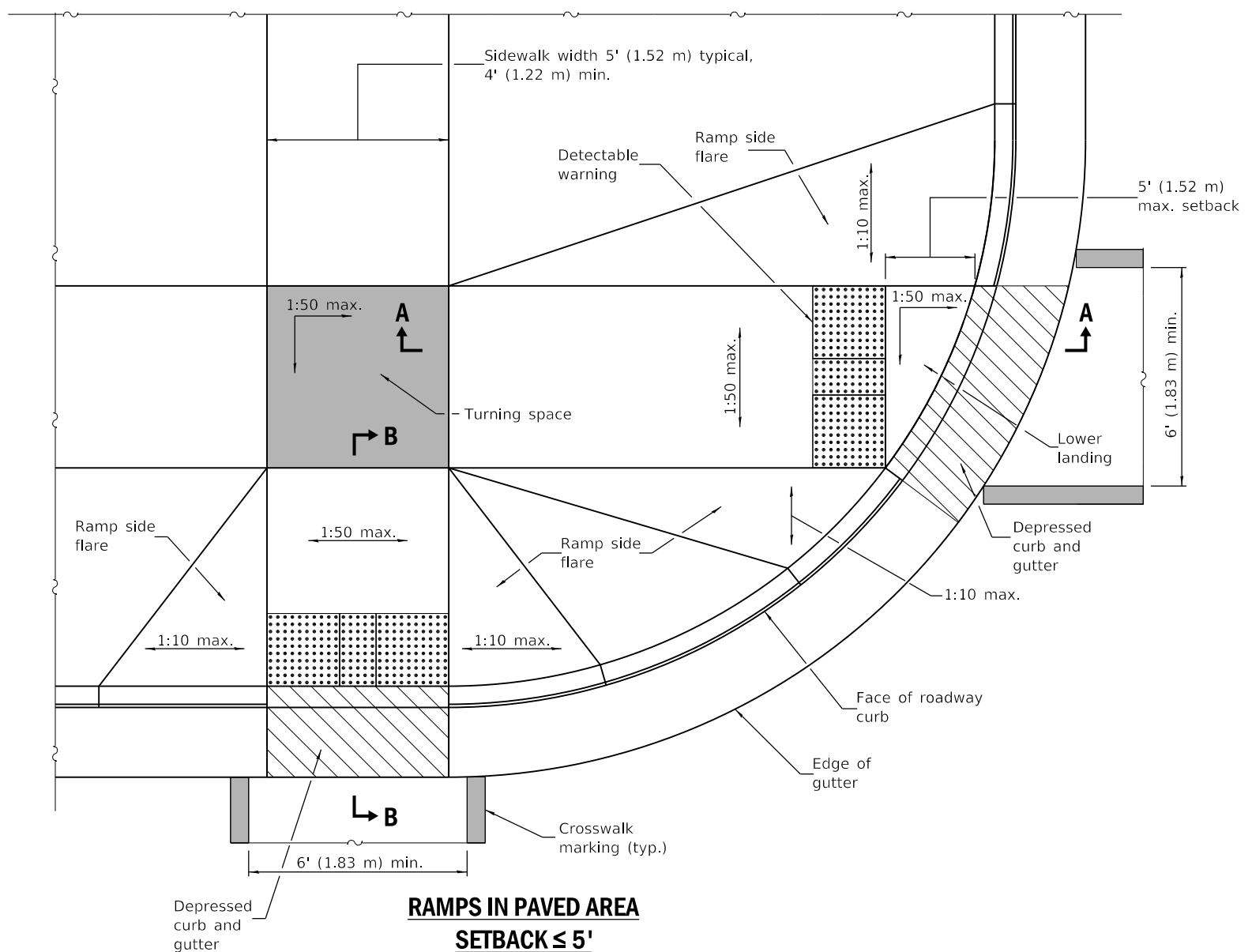


**RAMPS IN LANDSCAPED AREA  
SETBACK ≤ 5'**

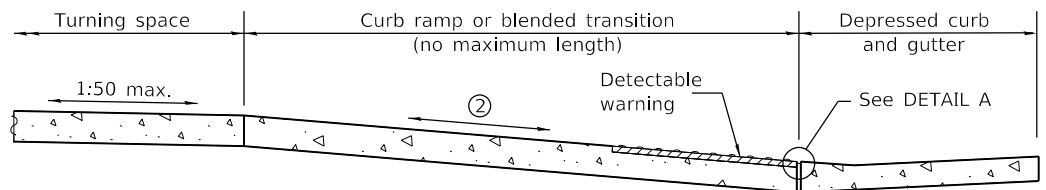


**SECTION A-A**

② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

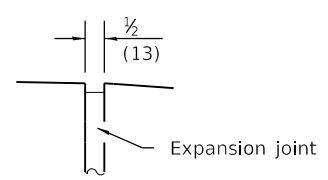


**RAMPS IN PAVED AREA  
SETBACK ≤ 5'**

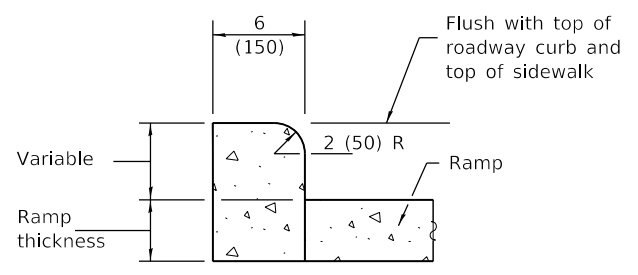


**SECTION B-B**

② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



**DETAIL A**



**SIDE CURB DETAIL**

Illinois Department of Transportation

PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2019  
*John E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

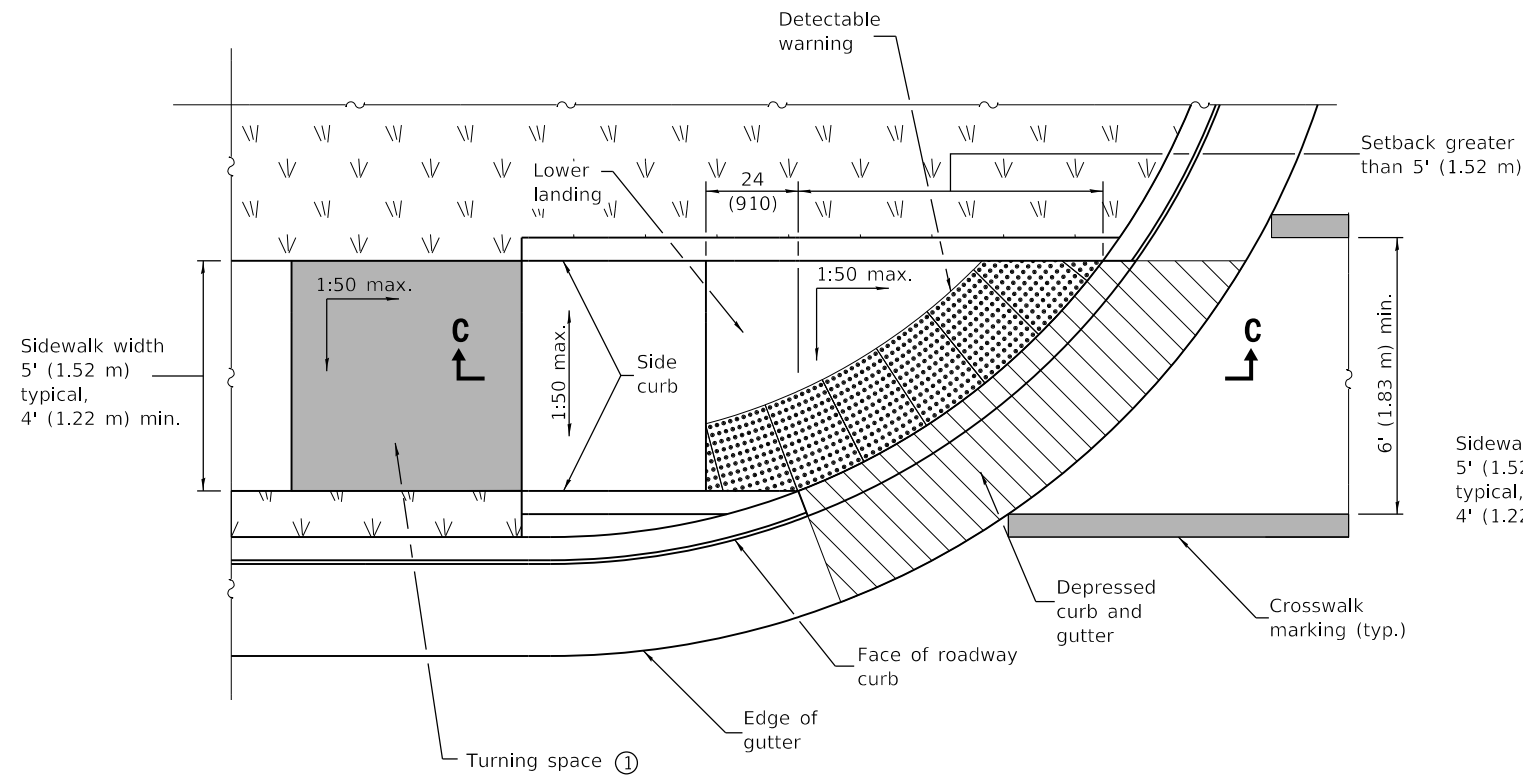
DATE	REVISIONS
1-1-19	Removed "15-foot rule", added "Blended transitions" and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at turning spaces and lower landings.

See Sheet 2 for GENERAL NOTES.

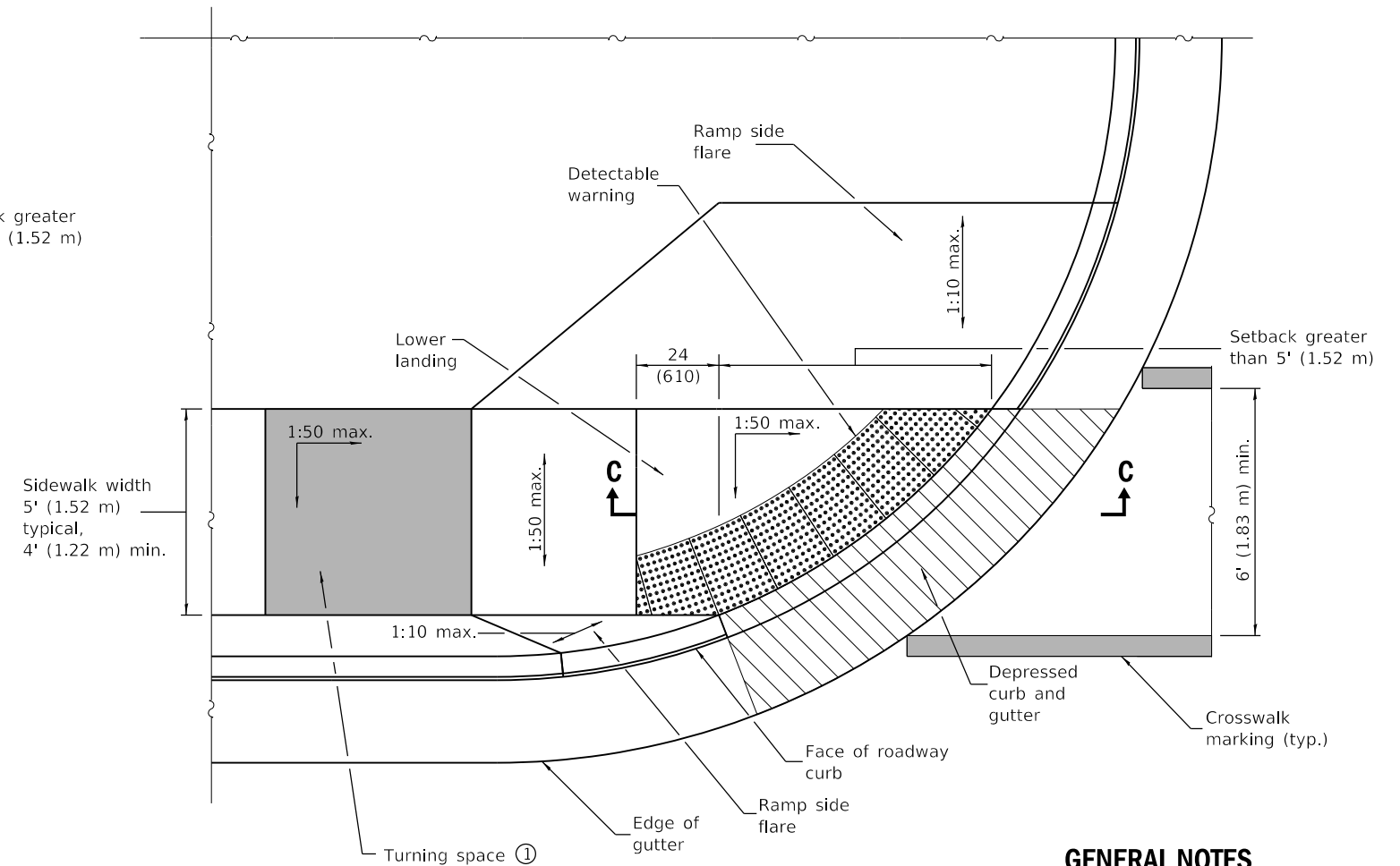
**PERPENDICULAR CURB RAMPS  
FOR SIDEWALKS**

(Sheet 1 of 2)

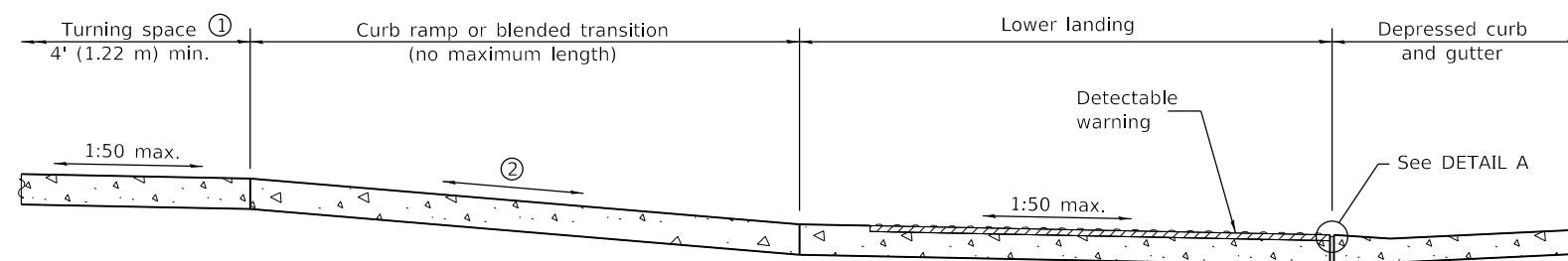
**STANDARD 424001-11**



**RAMP IN LANDSCAPED AREA  
SETBACK > 5'**



**RAMP IN PAVED AREA  
SETBACK > 5'**



**SECTION C-C**

- ① This turning space not required for blended transitions.
- ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

**GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

**Side Border** - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

**Curb Set-Back** - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

**PERPENDICULAR CURB RAMPS  
FOR SIDEWALKS**

(Sheet 2 of 2)

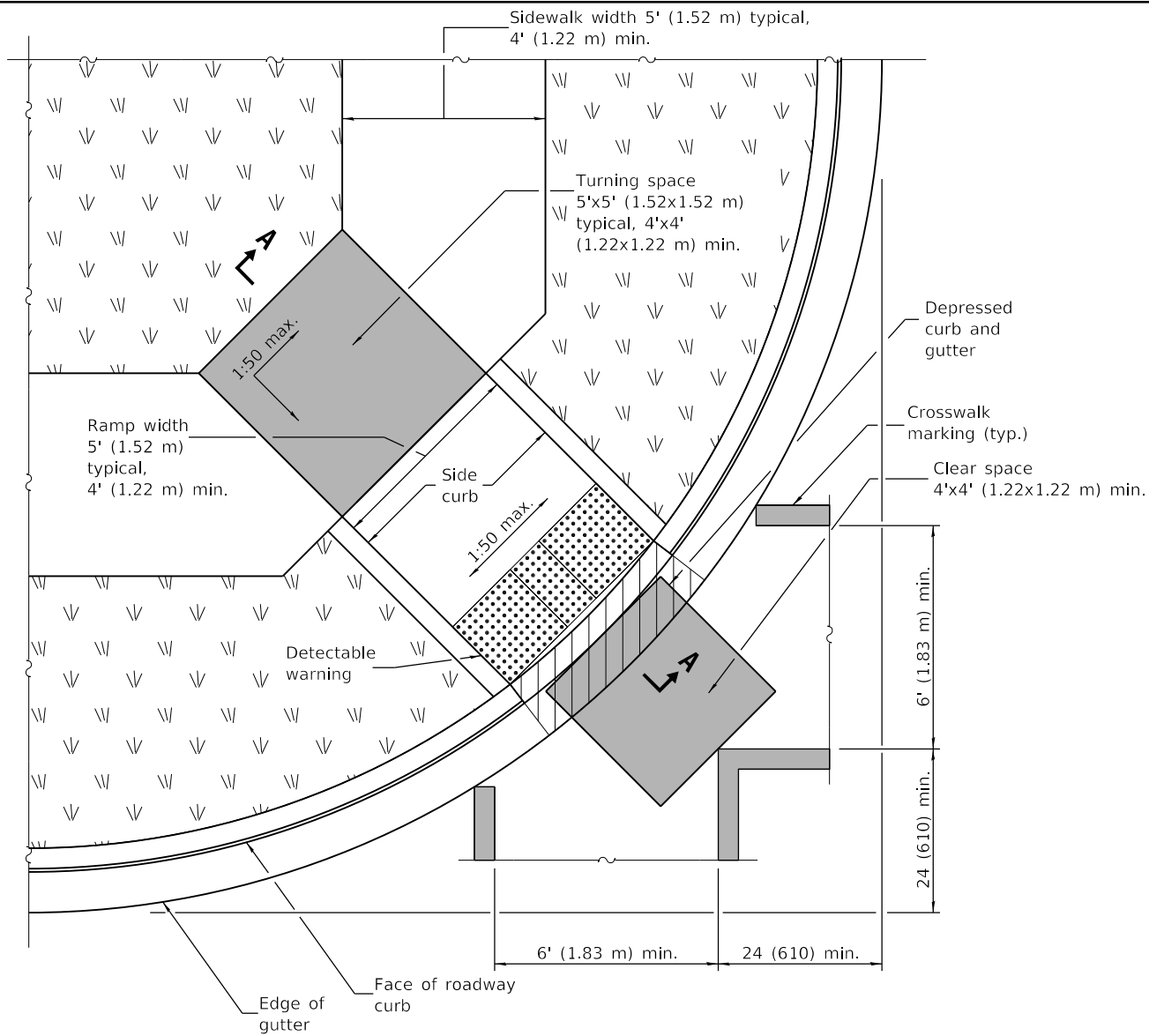
**STANDARD 424001-11**

Illinois Department of Transportation

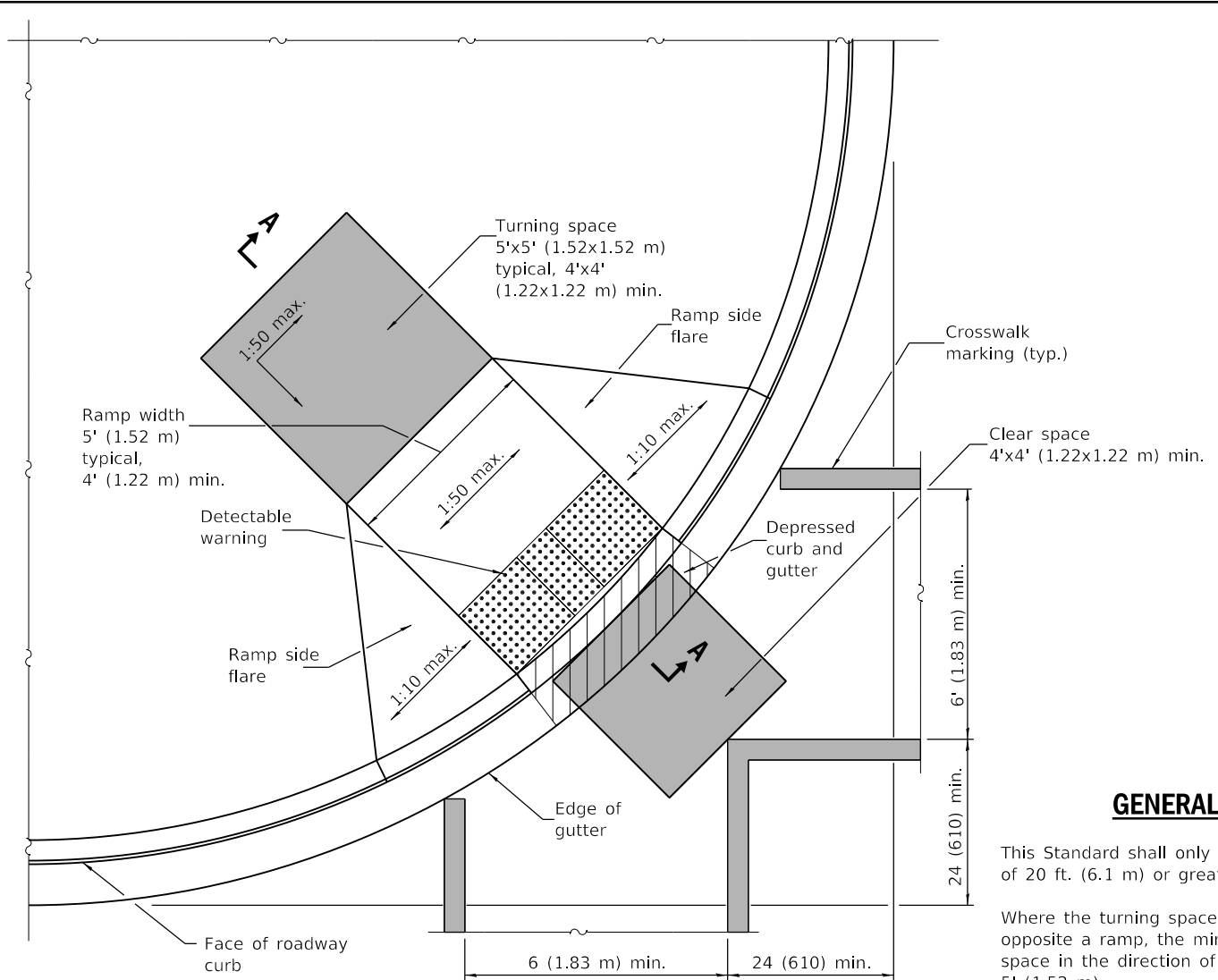
PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2019  
*Joe E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97



**RAMP IN LANDSCAPED AREA**



**RAMP IN PAVED AREA**

**GENERAL NOTES**

This Standard shall only be used for curb radii of 20 ft. (6.1 m) or greater.

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

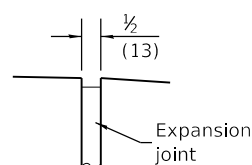
**Side Border** - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

**Curb Set-Back** - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

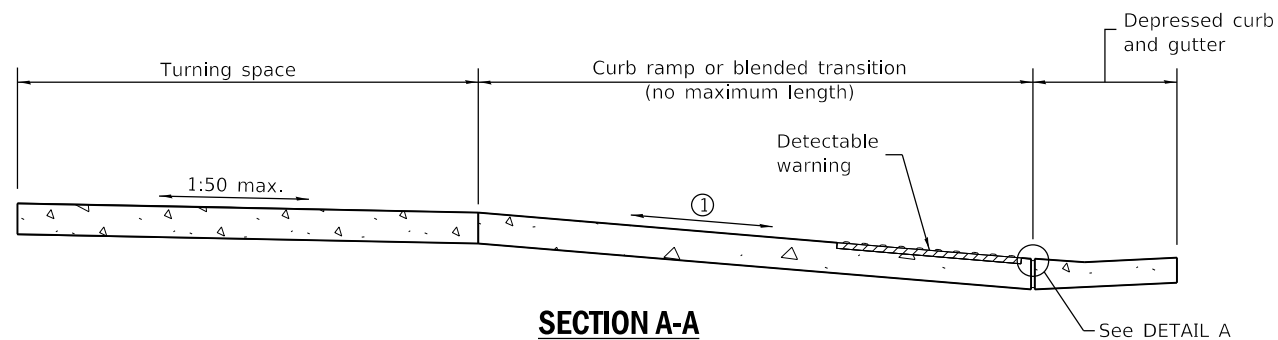
All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

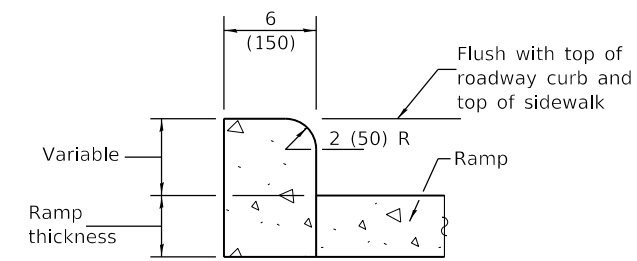


**DETAIL A**



**SECTION A-A**

① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



**SIDE CURB DETAIL**

DATE	REVISIONS
1-1-21	Clarified minimum crosswalk width and locations.
1-1-19	Removed "15-foot rule", added "blended transitions" and placement tolerances for detectable warnings.

**DIAGONAL CURB RAMPS FOR SIDEWALKS**

**STANDARD 424006-05**

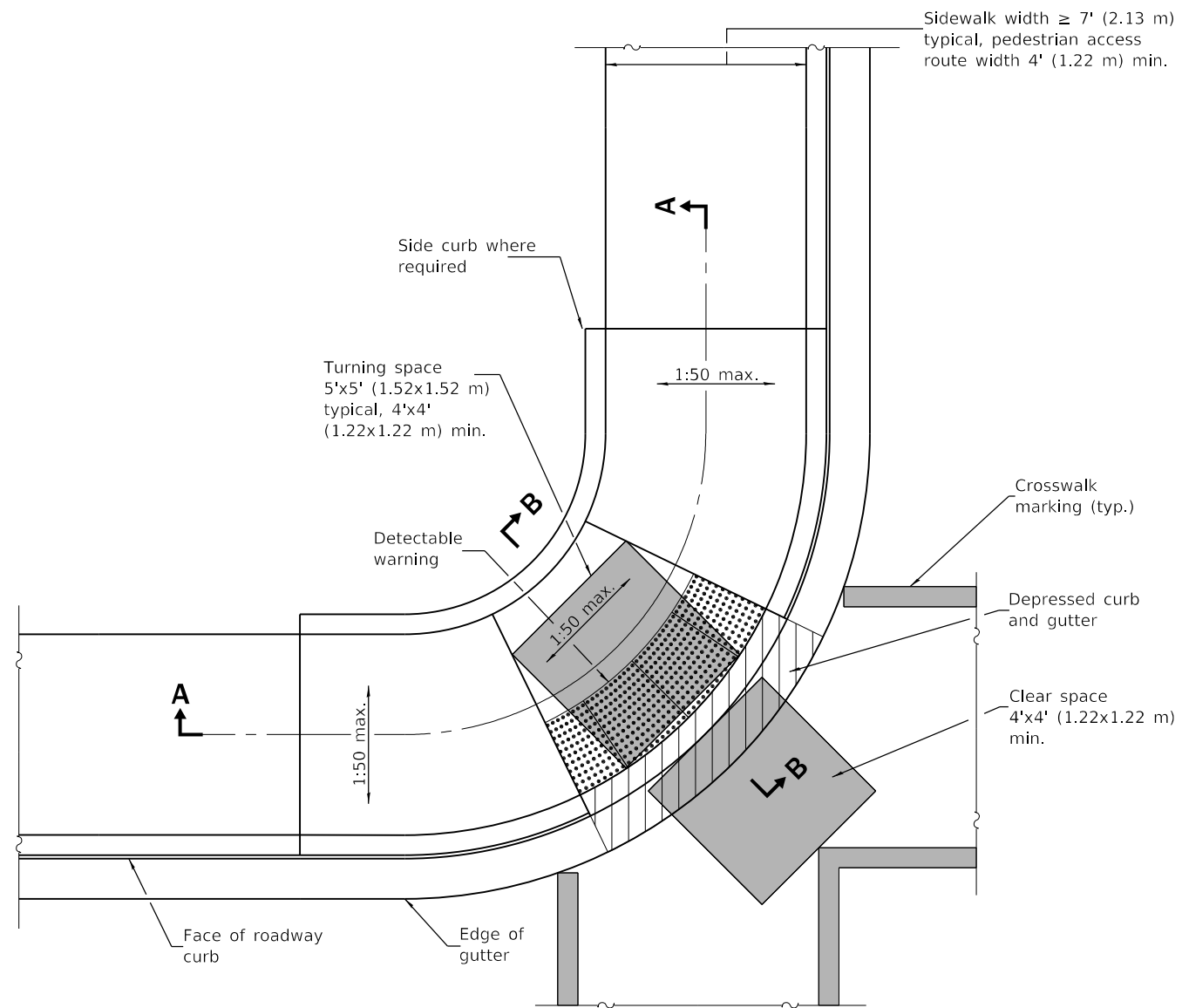
Illinois Department of Transportation

PASSED January 1, 2021  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

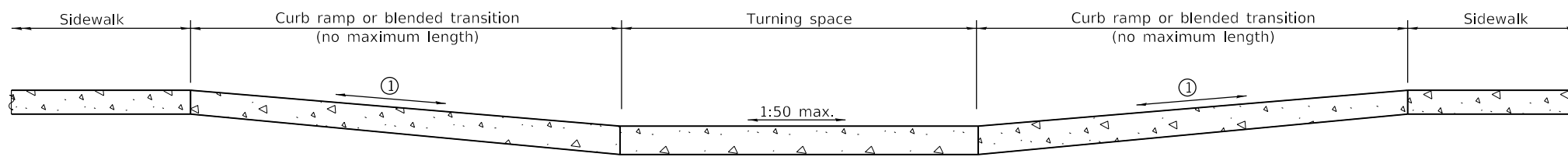
APPROVED January 1, 2021  
*John E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12



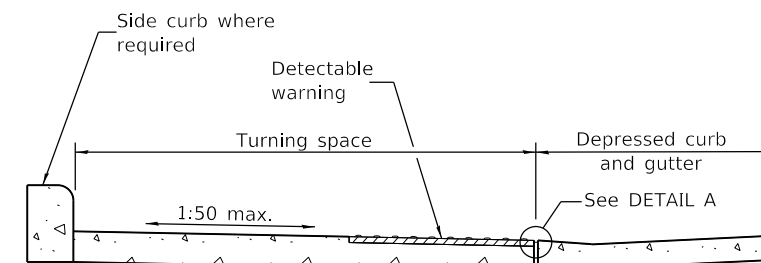


**CORNER PARALLEL CURB RAMP**

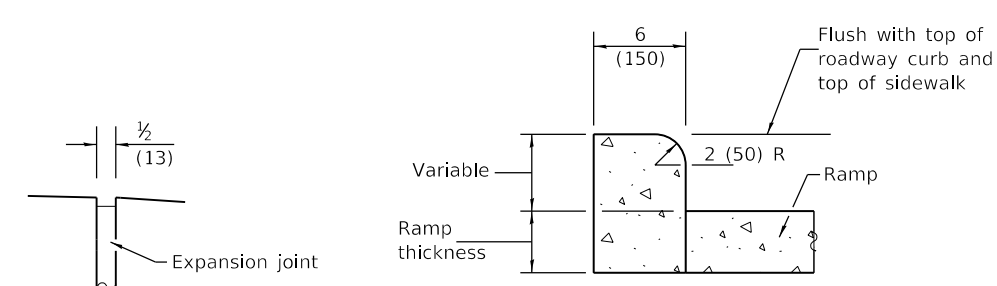


**SECTION A-A**

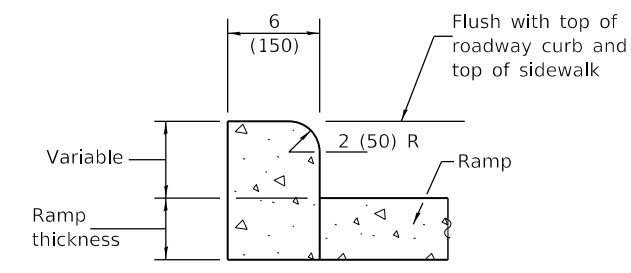
① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



**SECTION B-B**



**DETAIL A**



**SIDE CURB DETAIL**

**GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Removed upper landing, added blended transition and detectable warning tolerances.
1-1-17	Revised sidewalk width to include 24 (610) buffer behind curb.

**CORNER PARALLEL CURB RAMPS FOR SIDEWALKS**

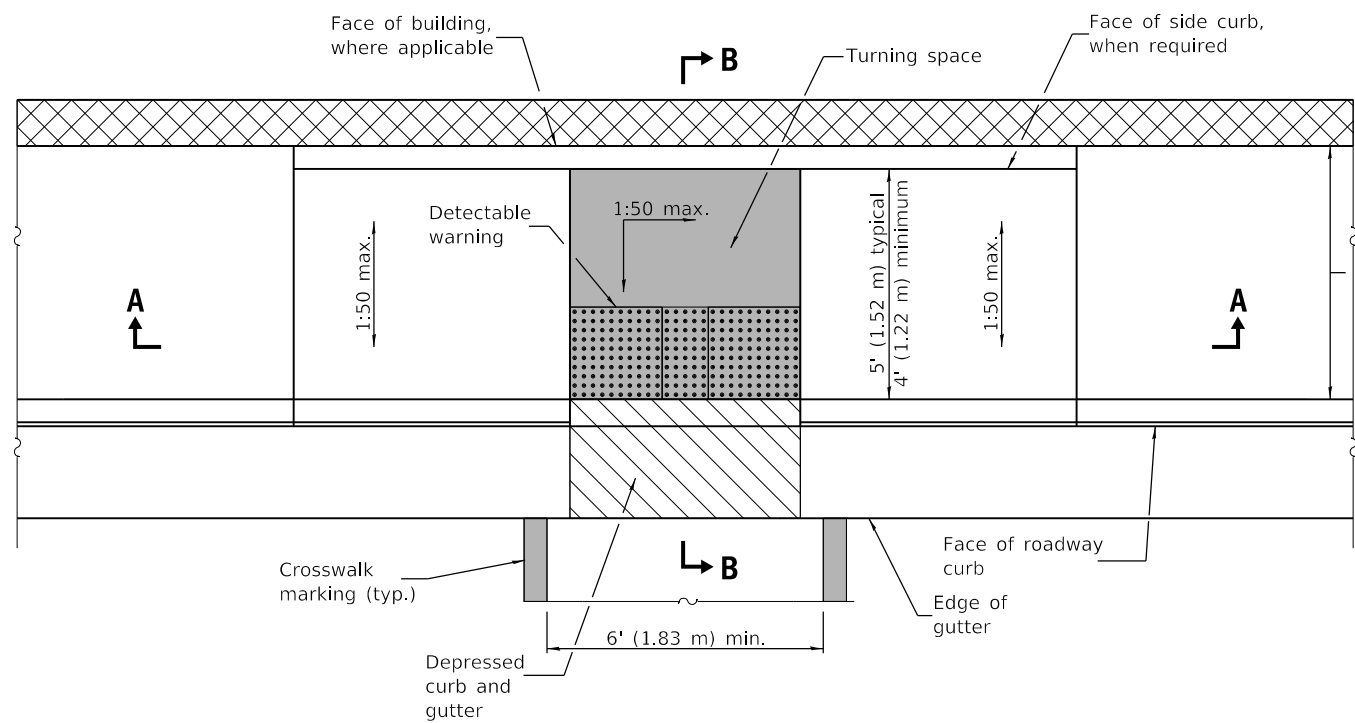
**STANDARD 424011-04**

Illinois Department of Transportation

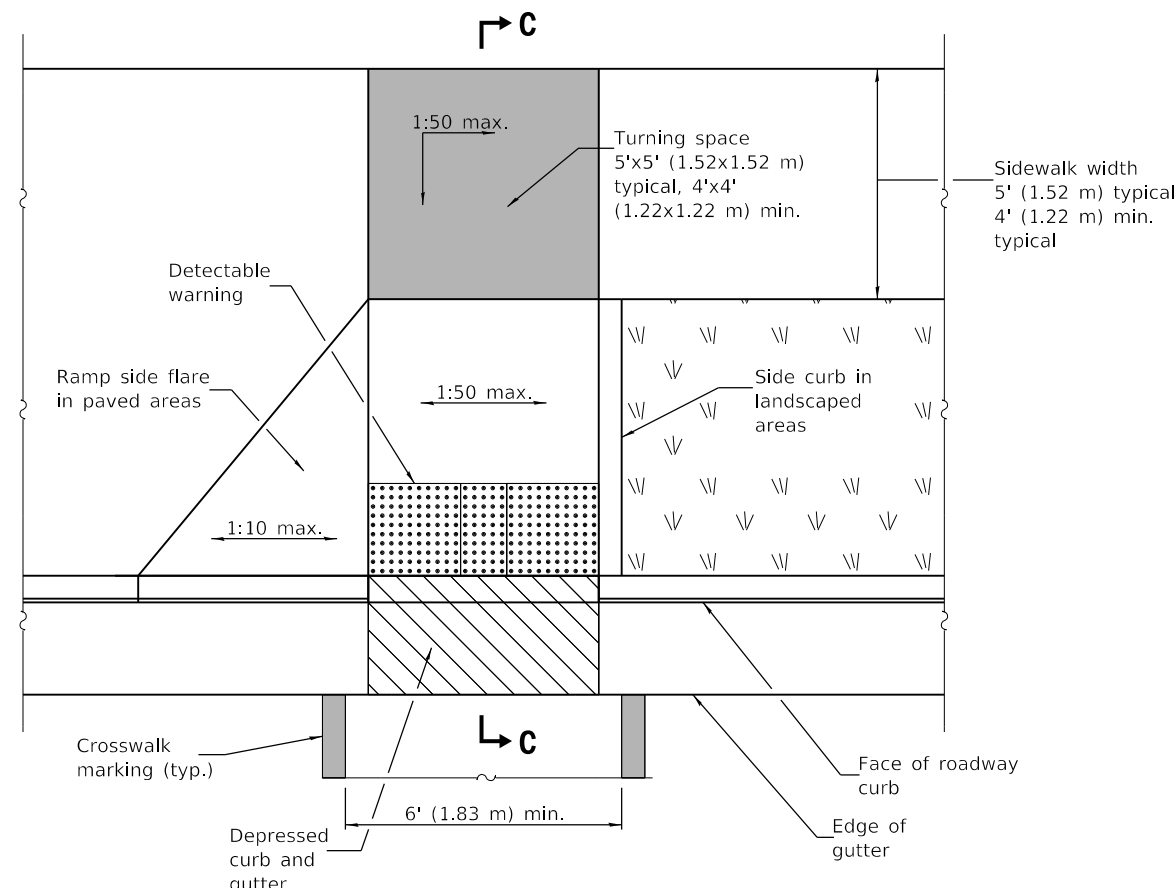
PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2019  
*John E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

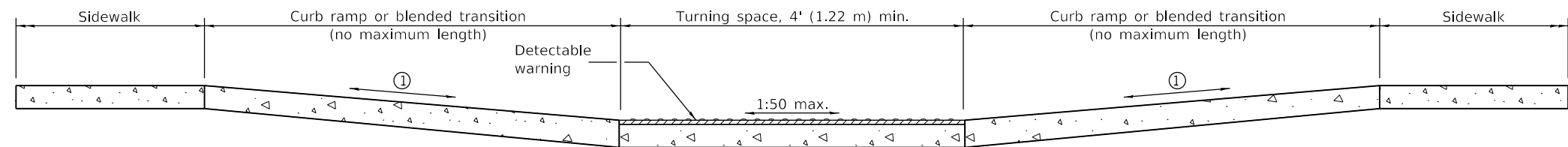
ISSUED 1-1-12



**PARALLEL MID-BLOCK CURB RAMP**

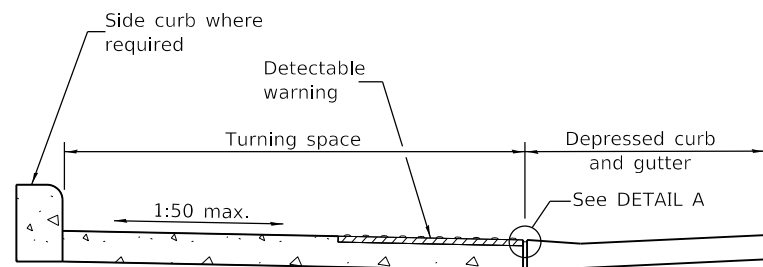


**PERPENDICULAR MID-BLOCK CURB RAMP**

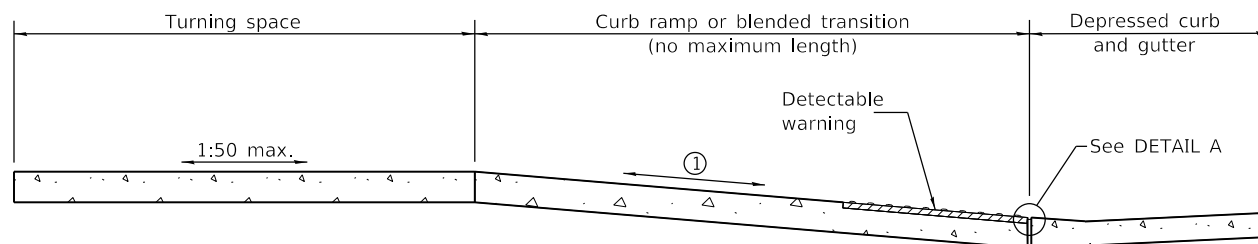


**SECTION A-A**

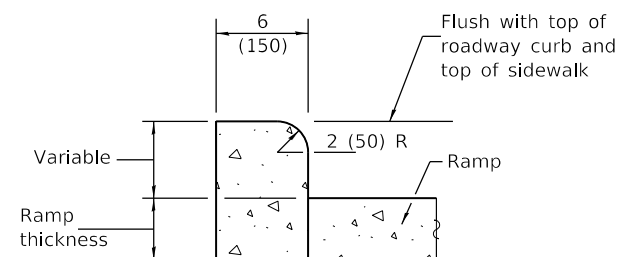
① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



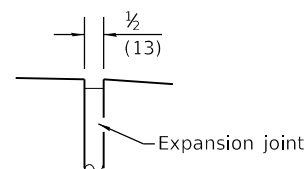
**SECTION B-B**



**SECTION C-C**



**SIDE CURB DETAIL**



**DETAIL A**

**GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in. width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Removed upper landing, added blended transitions and detectable warning tolerances.
1-1-18	Omitted diagonal slope at turning spaces and upper landings.

**MID-BLOCK CURB RAMPS FOR SIDEWALKS**

**STANDARD 424016-05**

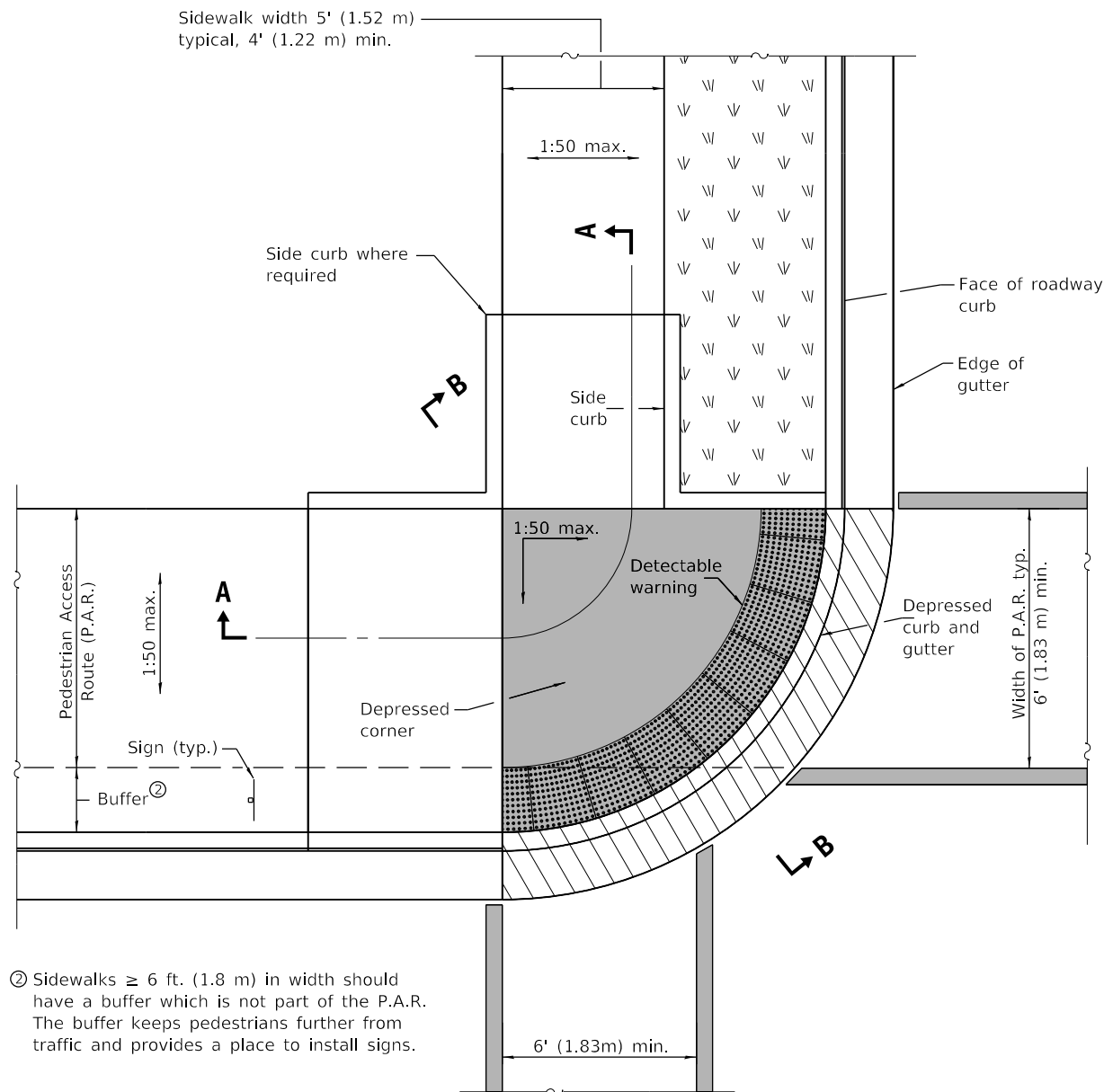
Illinois Department of Transportation

PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2019  
*John E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

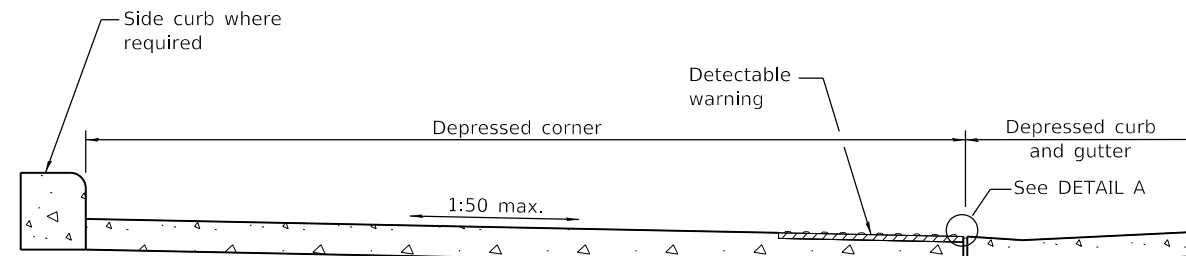
ISSUED 1-1-12

Sidewalk width 5' (1.52 m) typical, 4' (1.22 m) min.

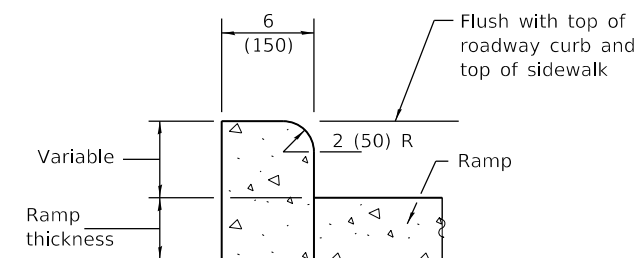


② Sidewalks  $\geq$  6 ft. (1.8 m) in width should have a buffer which is not part of the P.A.R. The buffer keeps pedestrians further from traffic and provides a place to install signs.

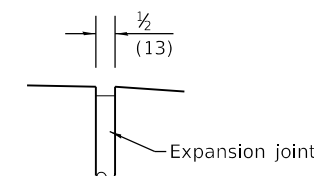
**DEPRESSED CORNER**



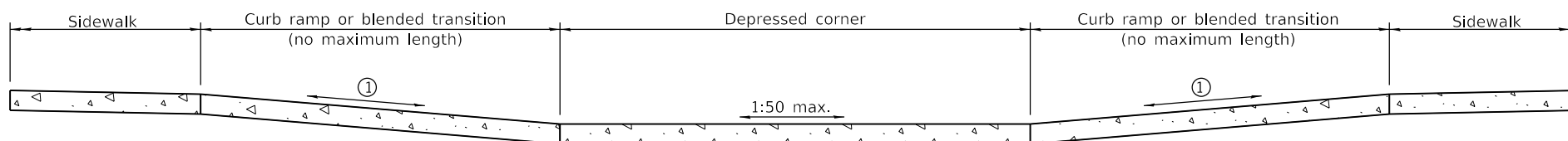
**SECTION B-B**



**SIDE CURB DETAIL**



**DETAIL A**



**SECTION A-A**

① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

**GENERAL NOTES**

This standard shall only be used for curb radii of 6 ft. (1.83 m) or greater.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal tolerances but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in. width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-21	Added crosswalk striping and a "buffer" for wide sidewalks.
1-1-19	Removed upper landings, added blended transition and detectable warning tolerances.

**DEPRESSED CORNER FOR SIDEWALKS**

**STANDARD 424021-06**

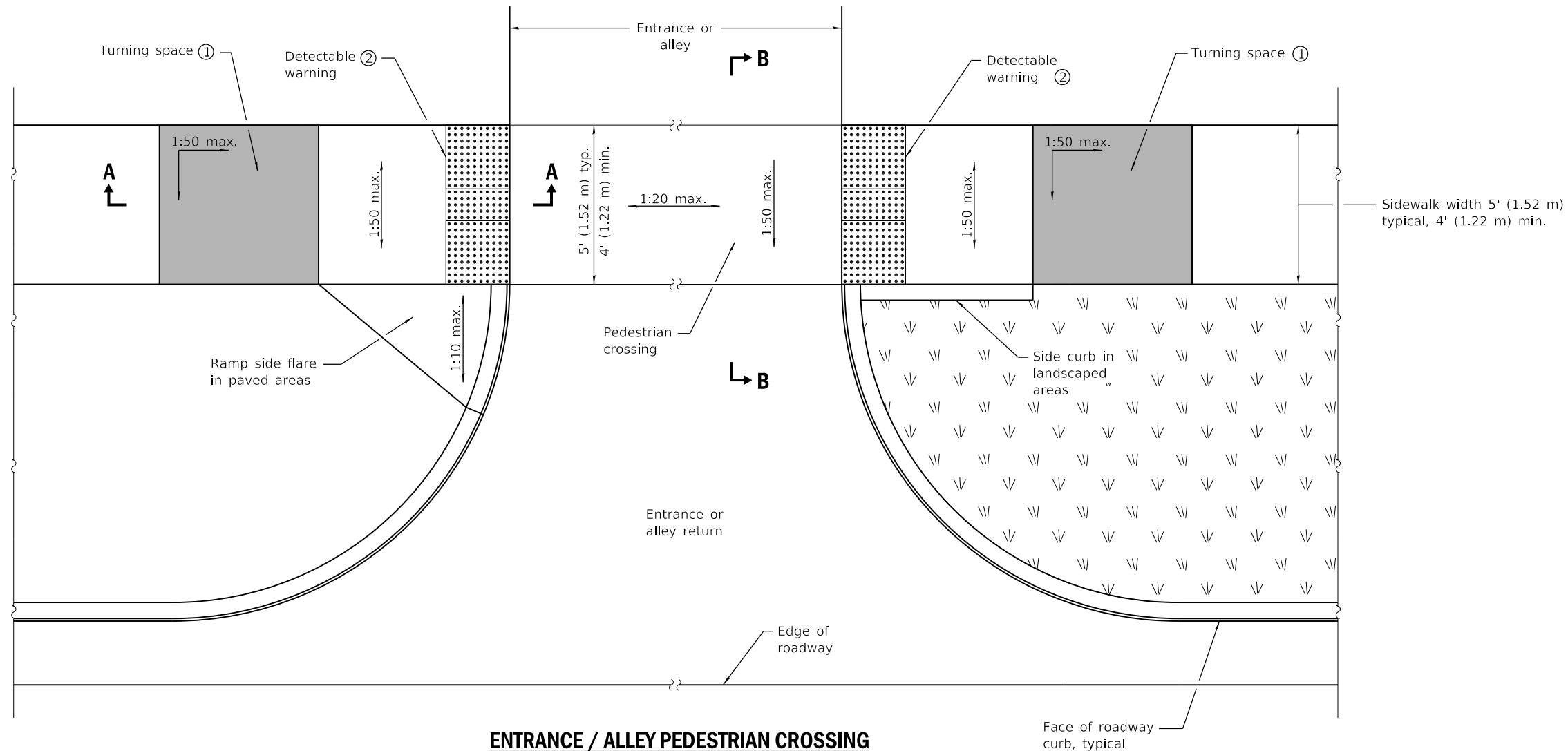
Illinois Department of Transportation

PASSED January 1, 2021  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

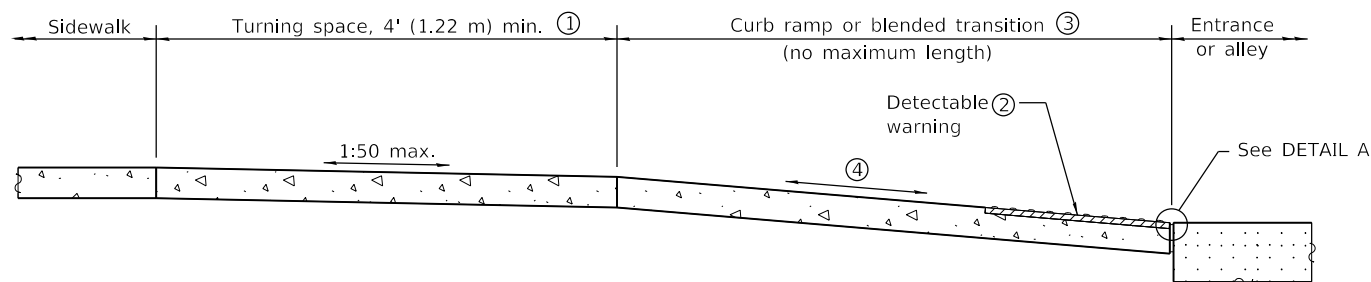
APPROVED January 1, 2021  
*J. E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12

- ② Detectable warning shall only be installed at entrances/alleys with permanent traffic control devices (i.e. stop signs, signals).
- ③ Where possible, maintain the grade of the sidewalk across the entrance/alley to avoid the need for ramps and turning spaces.

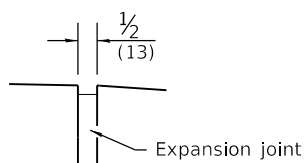


**ENTRANCE / ALLEY PEDESTRIAN CROSSING**

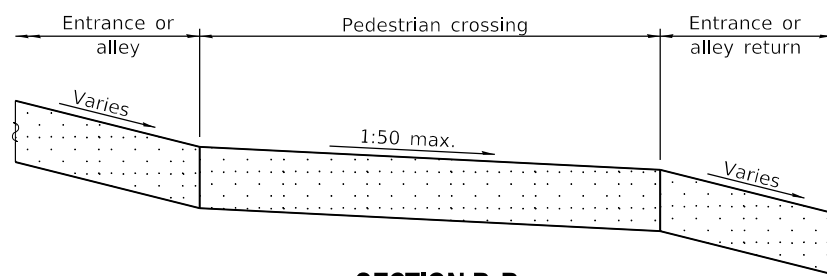


**SECTION A-A**

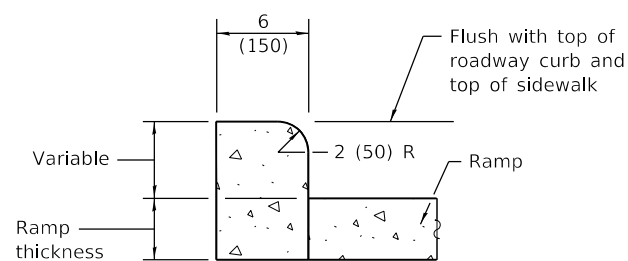
- ① Turning space not required for blended transitions.
- ④ The running slope of a curb ramp shall be 1:20 min and 1:12 max. The running slope of a blended transition shall be 1:20 max.



**DETAIL A**



**SECTION B-B**



**SIDE CURB DETAIL**

**GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Added blended transitions and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at upper landings.

**ENTRANCE / ALLEY PEDESTRIAN CROSSINGS**

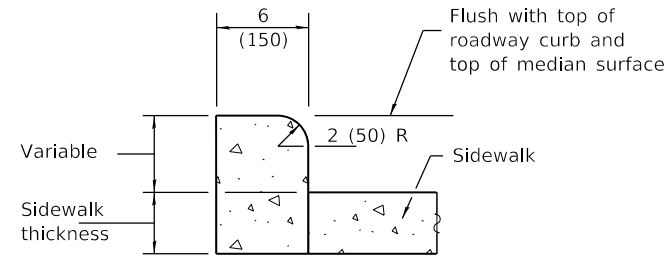
**STANDARD 424026-03**

Illinois Department of Transportation

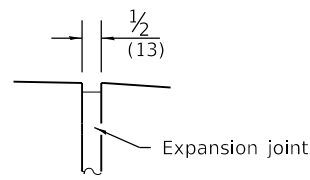
PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2019  
*Joe E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

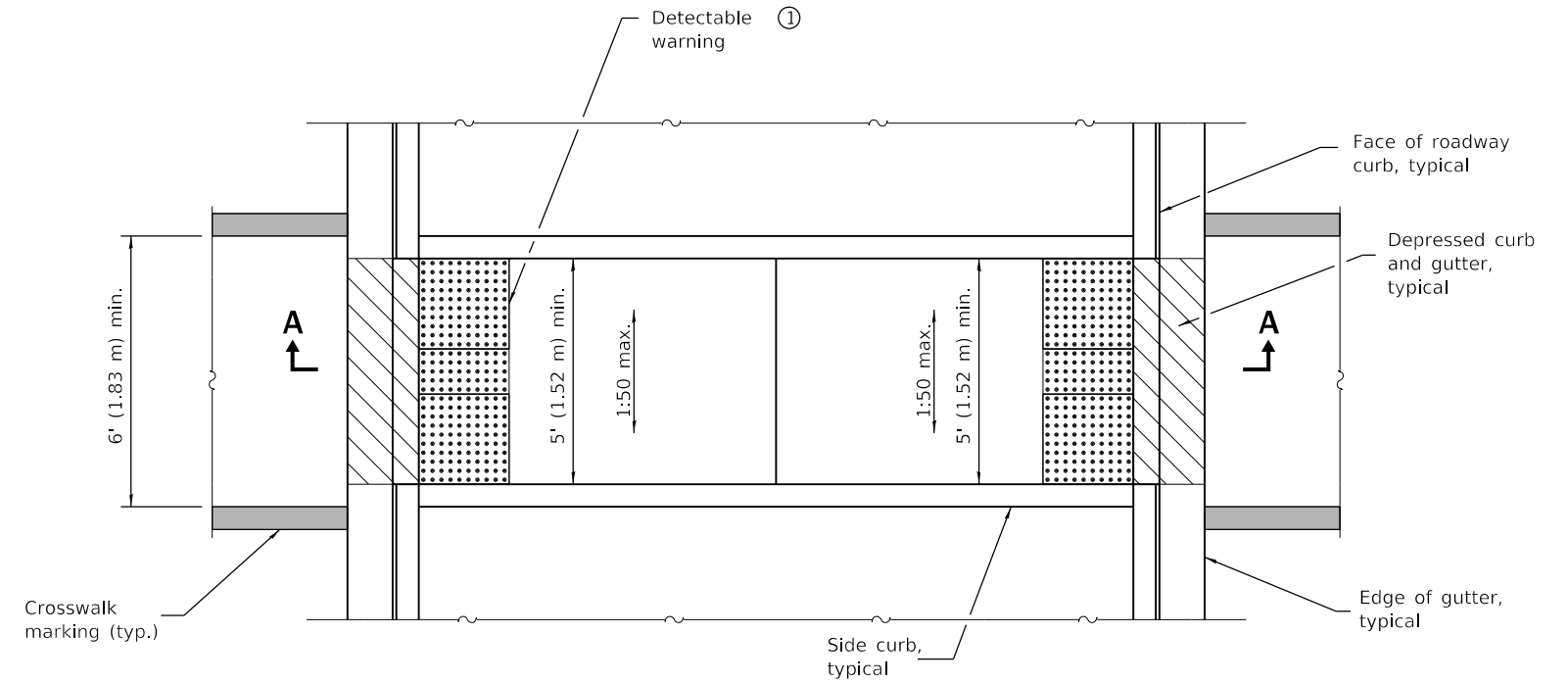
ISSUED 1-1-12



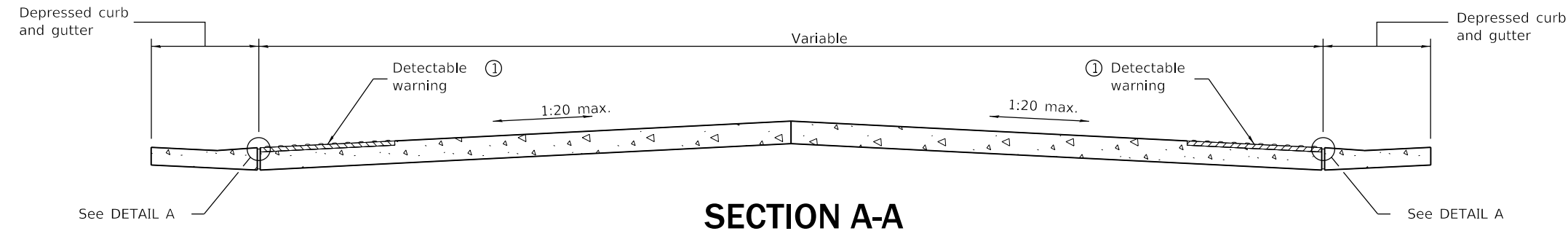
**SIDE CURB DETAIL**



**DETAIL A**



**MEDIAN PEDESTRIAN CROSSING**



**SECTION A-A**

① Omit detectable warnings when distance between back of curbs is less than 6' (1.83 m).

**GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

**Side Border** - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

**Curb Set-Back** - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

PASSED January 1, 2019  
*Michael Bond*  
 ENGINEER OF POLICY AND PROCEDURES

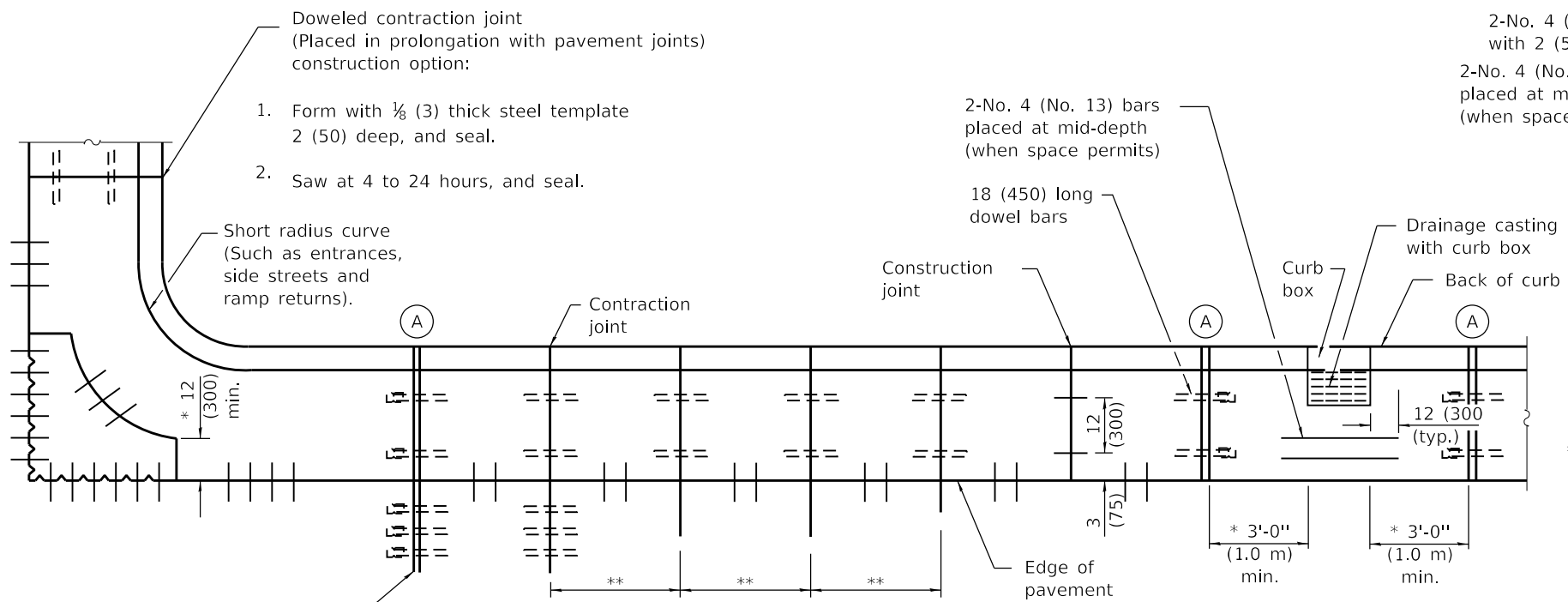
APPROVED January 1, 2019  
*John E. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12

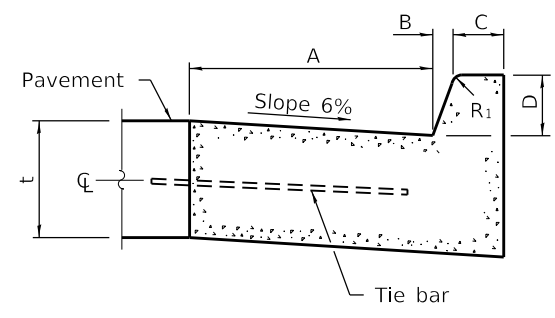
DATE	REVISIONS
1-1-19	Added placement tolerances for detectable warnings.
1-1-12	Widened crosswalk to 6' (1.83 m) min. inside dimension.
	Revised General Notes.

**MEDIAN PEDESTRIAN CROSSINGS**

**STANDARD 424031-02**



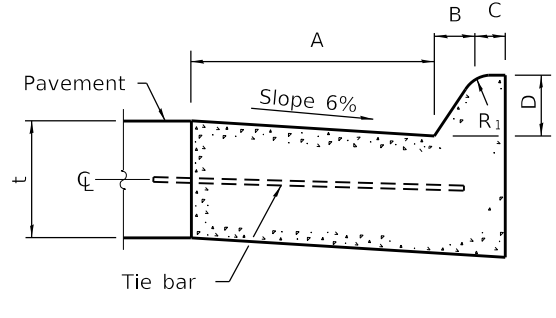
**PLAN**  
**ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE**



**BARRIER CURB**

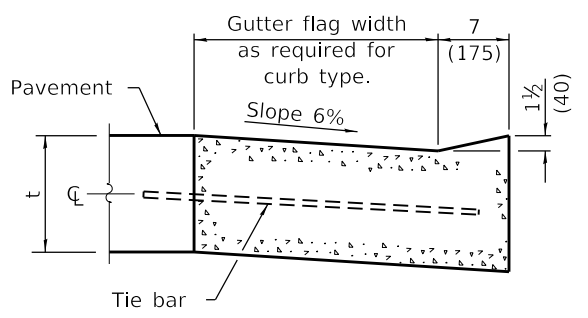
TABLE OF DIMENSIONS BARRIER CURB					
TYPE	A	B	C	D	R <sub>1</sub>
B-6.06 *	6	1	6	6	1
(B-15.15)	(150)	(25)	(150)	(150)	(25)
B-6.12	12	1	6	6	1
(B-15.3)	(300)	(25)	(150)	(150)	(25)
B-6.18	18	1	6	6	1
(B-15.45)	(450)	(25)	(150)	(150)	(25)
B-6.24	24	1	6	6	1
(B-15.60)	(600)	(25)	(150)	(150)	(25)
B-9.12	12	2	5	9	1
(B-22.30)	(300)	(50)	(125)	(225)	(25)
B-9.18	18	2	5	9	1
(B-22.45)	(450)	(50)	(125)	(225)	(25)
B-9.24	24	2	5	9	1
(B-22.60)	(600)	(50)	(125)	(225)	(25)

\* For corner islands only.

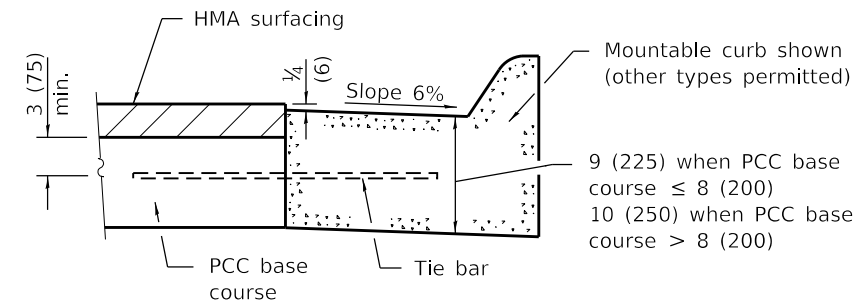


**MOUNTABLE CURB**

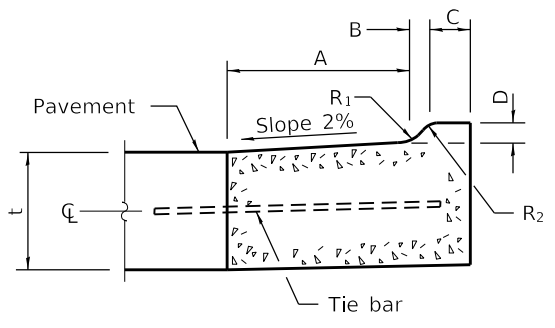
TABLE OF DIMENSIONS MOUNTABLE CURB						
TYPE	A	B	C	D	R <sub>1</sub>	R <sub>2</sub>
M-2.06	6	2	4	2	3	2
(M-5.15)	(150)	(50)	(100)	(50)	(75)	(50)
M-2.12	12	2	4	2	3	2
(M-5.30)	(300)	(50)	(100)	(50)	(75)	(50)
M-4.06	6	4	3	4	3	NA
(M-10.15)	(150)	(100)	(75)	(100)	(75)	NA
M-4.12	12	4	3	4	3	NA
(M-10.30)	(300)	(100)	(75)	(100)	(75)	NA
M-4.18	18	4	3	4	3	NA
(M-10.45)	(450)	(100)	(75)	(100)	(75)	NA
M-4.24	24	4	3	4	3	NA
(M-10.60)	(600)	(100)	(75)	(100)	(75)	NA
M-6.06	6	6	2	6	2	NA
(M-15.15)	(150)	(150)	(50)	(150)	(50)	NA
M-6.12	12	6	2	6	2	NA
(M-15.30)	(300)	(150)	(50)	(150)	(50)	NA
M-6.18	18	6	2	6	2	NA
(M-15.45)	(450)	(150)	(50)	(150)	(50)	NA
M-6.24	24	6	2	6	2	NA
(M-15.60)	(600)	(150)	(50)	(150)	(50)	NA



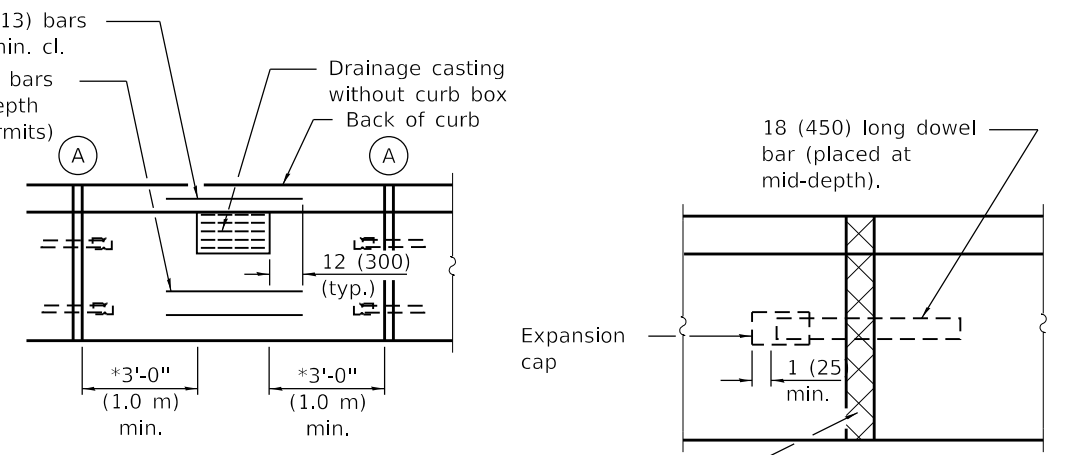
**DEPRESSED CURB (TYPICAL)**



**ADJACENT TO PCC BASE COURSE WITH HMA SURFACING**

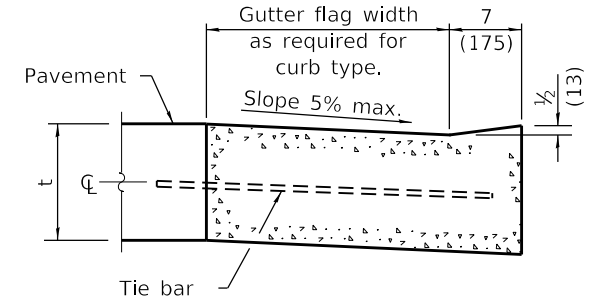


**M-2.06 (M-5.15) and M-2.12 (M-5.30)**



**DETAIL A**  
**EXPANSION JOINT**

Full depth & width 1 (25) - thick (min.) preformed expansion joint filler.



**DEPRESSED CURB ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED**

**GENERAL NOTES**

The bottom slope of combination curb and gutter constructed adjacent to pcc pavement shall be the same slope as the subbase or 6% when subbase is omitted.

t = Thickness of pavement.

Longitudinal joint tie bars shall be No. 6 (No. 19) at 36 (900) centers in accordance with details for longitudinal construction joint shown on Standard 420001.

A minimum clearance of 2 (50) between the end of the tie bar and the back of the curb shall be maintained.

The dowel bars shown in contraction joints will only be required for monolithic construction.

See Standard 606301 for details of corner islands.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-22	Revised contraction joint spacing adjacent to pcc pavement.
1-1-18	Revised General Note for tie bar spacing to 36 (900) cts.

**CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER**  
(Sheet 1 of 2)

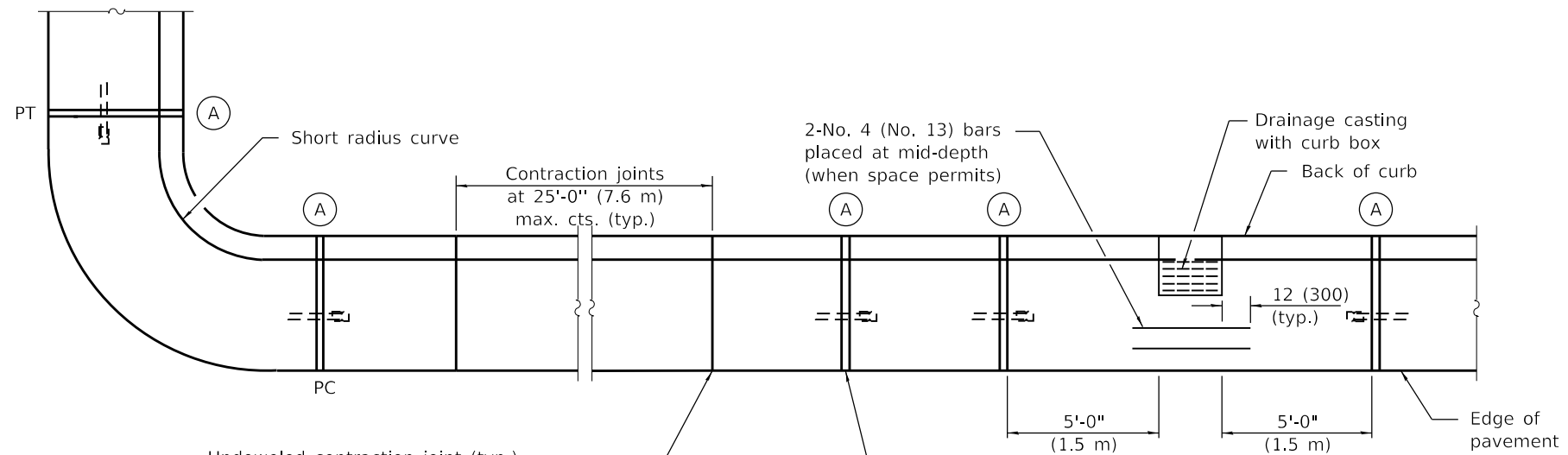
**STANDARD 606001-08**

Illinois Department of Transportation

PASSED January 1, 2022  
Michael Brand  
ENGINEER OF POLICY AND PROCEDURES

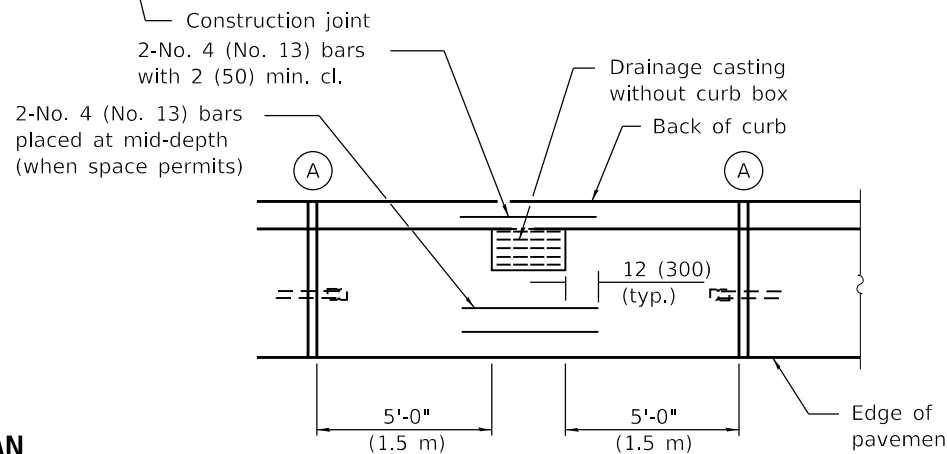
APPROVED January 1, 2022  
[Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

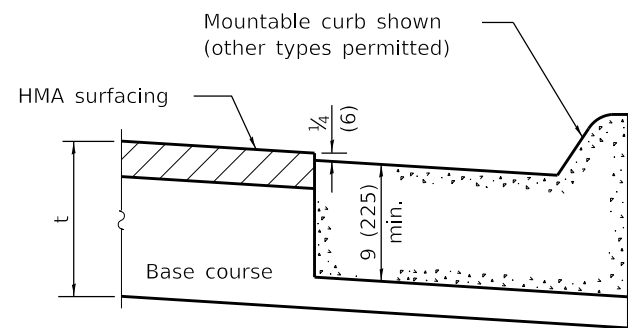


Undoweled contraction joint (typ.) construction options:

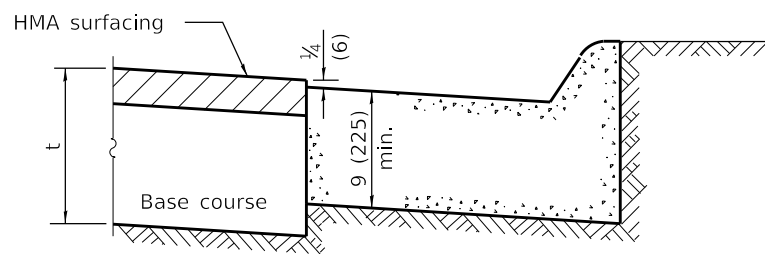
1. Form with 1/8 (3) thick steel template 2 (50) deep, and seal.
2. Saw 2 (50) deep at 4 to 24 hours, and seal.
3. Insert 3/4 (20) thick preformed joint filler full depth and width.



**PLAN**

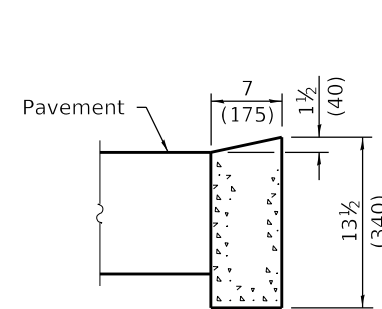


**ON DISTURBED SUBGRADE**

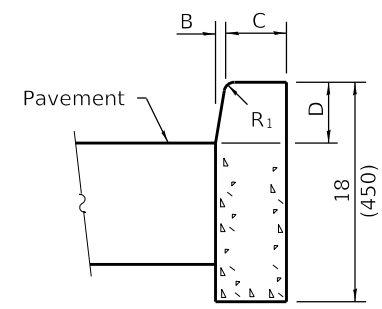


**ON UNDISTURBED SUBGRADE**

**ADJACENT TO FLEXIBLE PAVEMENT**

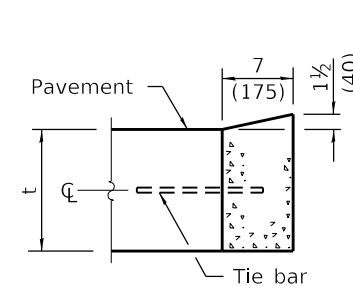


**DEPRESSED CURB**

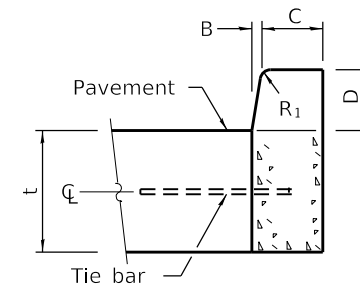


**BARRIER CURB**

**ADJACENT TO FLEXIBLE PAVEMENT**



**DEPRESSED CURB**



**BARRIER CURB**

**ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE**

**CONCRETE CURB TYPE B**

**CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER**

(Sheet 2 of 2)

**STANDARD 606001-08**

Illinois Department of Transportation

PASSED January 1, 2022  
*Michael Beard*  
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2022  
*John C. ...*  
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

# EXHIBIT E

## BUREAU OF LOCAL ROADS AND STREETS DESIGN STANDARDS



## **41-6 REQUIREMENTS FOR ACCESSIBLE PUBLIC RIGHTS-OF-WAY**

### **41-6.01 General**

Many highway elements can affect the accessibility and mobility of individuals with disabilities. These include sidewalks, curb ramps, parking spaces, buildings at transportation facilities, overpasses or underpasses, temporary traffic control, and landscaping or streetscaping. According to the *Americans with Disabilities Act* (ADA), 42 U.S.C. §12181 - 12189 and the *Illinois Environmental Barriers Act*, 410 ILCS 25/1 *et seq*, public highway right-of-way shall be made accessible to all users. Local Public Agencies (LPAs) are required to perform a self-assessment to determine barriers to accessibility within the public highway right-of-way.

Currently, a national uniform design guideline for public highway right-of-way has not been adopted by the United States Department of Justice and Federal Highway Administration (FHWA). However, the *Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG)* has been introduced by the United States Access Board through the federal rule-making process. Furthermore, FHWA has issued guidance that PROWAG may be used in order to comply with accessibility requirements. Therefore, the Department recommends that LPAs use the latest revision of PROWAG available at [www.access-board.gov](http://www.access-board.gov), when constructing or altering facilities within the highway public right-of-way.

Figure 41-6A provides a reference to criteria contained in PROWAG unless otherwise noted. The *ADA Standards for Accessible Designs* (ADAAG), the *Illinois Accessibility Code* (IAC), the *Uniform Federal Accessibility Standards* (UFAS), the *ILMUTCD*, and the *American National Standards Institute* (ANSI) should also be consulted as needed. Where LPA policies or local codes require criteria that exceed PROWAG, then the local criteria should be used.

### **41-6.02 Equivalent Facilitation**

The use of alternative designs, products, or technologies, that result in substantially equivalent or greater accessibility and usability than the requirements in PROWAG, is permitted. However, the LPA should document how the equivalent facilitation was determined.

### **41-6.03 Application**

All newly constructed facilities, altered portions of existing facilities, and elements added to existing facilities for pedestrian circulation and use located in the public right-of-way shall comply with the requirements in this Section.

**BUREAU OF LOCAL ROADS & STREETS**  
**SPECIAL DESIGN ELEMENTS**

41-6(2)

Oct 2013

Accessible Component	Scoping Requirements	Technical Requirements
Pedestrian Access Route (PAR)	R204	R302
Alternate PAR	R205	
Pedestrian Street Crossing	R206	R306
Curb Ramps and Blended Transitions	R207	R304
Detectable Warnings	R208	R305
Accessible Pedestrian Signals and Pedestrian Pushbuttons	R209	
Protruding Objects	R210	R402
Signs	R211	R410
Transit Stops and Transit Shelters	R213	R308
On-Street Parking	R214	R309
Off-Street Parking	ADAAG R208	ADAAG R502; IAC 400.310(c)
Passenger Loading Zones	R215	R310
Stairways and Escalators	R216	R408
Handrails	R217	R409
Doors, Doorways, and Gates	R218	
Ramps		R407

**SCOPING AND TECHNICAL REQUIREMENTS**

**Figure 41-6A**

**41-6.04 Alterations**

**41-6.04(a) General**

Where existing elements, spaces, or facilities are altered, each altered element, space, or facility within the scope of the project shall comply with the applicable requirements for new construction unless existing physical constraints exist. The alteration of multiple elements or spaces within a facility may provide a cost-effective opportunity to make the entire facility or a significant portion of the facility accessible.

Figure 41-6B provides a summary of some common alterations.

**BUREAU OF LOCAL ROADS & STREETS**  
**SPECIAL DESIGN ELEMENTS**

Oct 2013

41-6(3)

Type of Work	Alteration Required
Resurfacing (includes, but not limited to, addition of a new layer of asphalt, reconstruction, concrete pavement rehabilitation and reconstruction, open-graded surface course, microsurfacing and thin lift overlays, cape seals, and in-place asphalt recycling)	Correct non-compliant curb ramps and crosswalks
Remove and Replace Sidewalk (sidewalk patch, relocate poles or posts, etc.)	Replace with compliant (includes transition to non-compliant)
Curb & Gutter Replacement	Install curb ramps if replaced curb and gutter is at a pedestrian crossing with adjacent sidewalk
Pedestrian Signal (signal controller and software altered, or signal head replaced)	Installed Accessible Pedestrian Signals and accessible pushbutton
Restriping Parking Markings	Provide accessible spaces

**ALTERATION REQUIRED BY TYPE OF WORK**

**Figure 41-6B**

**41-6.04(b) Existing Physical Constraints.**

Where existing physical constraints make it impracticable for altered elements, spaces, or facilities to fully comply with the requirements for new construction, compliance is required to the extent practicable within the scope of the project. Existing physical constraints include, but are not limited to, underlying terrain, right-of-way availability, underground structures, adjacent developed facilities, drainage, or the presence of a notable natural or historic feature.

The LPA should document the various options considered, reasons for selecting, and any public comments received concerning an alteration to the extent practicable. Furthermore, the facility shall continue to be listed as a barrier in the LPA's self-evaluation.

**41-6.04(c) Transitional Segments.**

Transitional segments of an altered PARs shall connect to existing unaltered segments of pedestrian circulation paths. The transitional segment shall be removed when the existing unaltered section is altered in the future.

**41-6.04(d) Reduction in Access Prohibited.**

An alteration shall not decrease or have the effect of decreasing the accessibility of a facility or an accessible connection to an adjacent building or site below the requirements for new construction in effect at the time of the alteration. Sidewalk improvements that correct existing excessive cross slope should be carefully planned to avoid creating excessive slope in curb ramps or adding a step at existing building entrances. Solutions may include:

1. Split sidewalks that serve building entrances and street or highway at separate levels;

2. Sidewalks with greater cross slope along the curb and PARs with lesser cross slope along building fronts;
3. PARs along the curb and ramped entrances to buildings.

**41-6.04(e) Alterations to Qualified Historic Facilities.**

Where the State Historic Preservation Officer or Advisory Council on Historic Preservation determines that compliance with a requirement would threaten or destroy historically significant features of a qualified historic facility, compliance shall be required to the extent that it does not threaten or destroy historically significant features of the facility.

Where there is a federal agency “undertaking”, as defined in 36 CFR 800.16 (y), the requirements in section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and 36 CFR part 800 apply. Location of a facility within an historic district by itself does not excuse compliance with the requirements in this document. The State Historic Preservation Officer or Advisory Council on Historic Preservation must determine that compliance would threaten or destroy historically significant features of the facility. Reproductions or replications of historic facilities are not qualified historic facilities.

**41-6.05 Pedestrian Access Route**

A PAR shall be provided within sidewalks and other pedestrian circulation paths located in the public right-of-way. The pedestrian access route shall connect to accessible elements, spaces, facilities, and to other accessible routes not within the public right-of-way.

PARs shall consist of one or more of the following: sidewalks and other pedestrian circulation paths, or a portion of sidewalks and other pedestrian circulation paths; pedestrian street crossings and at-grade rail crossings; pedestrian overpasses and underpasses and similar structures; curb ramps and blended transitions; ramps; elevators and limited use/limited application elevators; platform lifts; and doors, doorways, and gates.

**41-6.06 Alternate Pedestrian Access Route**

When a pedestrian circulation path is temporarily closed by construction, alterations, maintenance operations, or other conditions, an alternate PAR shall be marked and/or provided. Any pedestrian barricades and channelizing devices shall comply with the *ILMUTCD*.

The *ILMUTCD* recommends that whenever possible, work should be done in a manner that does not create a need to detour pedestrians from existing pedestrian routes. Extra distance and additional pedestrian street crossings add complexity to a trip and increase exposure of risk to accidents.

#### **41-6.07 Curb Ramps and Blended Transitions**

A curb ramp, blended transition, or a combination of curb ramps and blended transitions shall be installed to allow pedestrians to continue through pedestrian street crossings, alleys, or driveways. The curb ramp (excluding any flared sides) or blended transition shall be contained wholly within the width of the crossing served. In alterations where existing physical constraints prevent a curb ramp at each crossing, a single diagonal curb ramp shall be permitted to serve both pedestrian street crossings.

There are two types of curb ramps:

- Perpendicular curb ramps have a running slope that cuts through or is built up to the curb at right angles, or meets the gutter break at right angles where the curb is curved. On large corner radiuses, it will be necessary to indent the gutter break on one side of the curb ramp in order for the curb ramp to meet the gutter break at right angles.
- Parallel curb ramps have a running slope that is in-line with the direction of sidewalk travel and lower the sidewalk to a level turning space where a turn is made to enter the pedestrian street crossing.
- Parallel and perpendicular curb ramps may be combined. A parallel curb ramp is used to lower the sidewalk to a mid-landing and a short perpendicular curb ramp connects the landing to the street.

Blended transitions are similar to curb ramps. However, blended transitions have a less severe running slope and some other differences from curb ramp design. Raised pedestrian street crossings are considered blended transitions. Blended transitions are suitable for a range of sidewalk conditions and should be considered at level intersections.

The Department has developed a series of highway standards that provide the requirements for curb ramp and/or blended transition construction. However, these standards may not provide the contractor and LPA construction supervisor with enough information to ensure accessibility compliance at locations with severe slopes, existing site constraints, or other unique situations. Therefore, the LPA should consider providing plan details for each corner where curb ramps or blended transitions are being constructed.

#### **41-6.08 Detectable Warning Surfaces**

##### **41-6.08(a) General**

Detectable warning surfaces indicate the boundary between pedestrian and vehicular routes where there is a flush rather than a curbed connection and shall be provided at the following locations on the PAR and at transit stops:

- Curb ramps and blended transitions at pedestrian street crossings;
- Pedestrian refuge islands (minimum width 6 ft);
- Pedestrian at-grade rail crossings not located within a street or highway;
- Boarding platforms at transit stops for buses and rail vehicles where the edges of the boarding platform are not protected by screens or guards; and

- Boarding and alighting areas at sidewalk or street level transit stops for rail vehicles where the side of the boarding and alighting areas facing the rail vehicles is not protected by screens or guards.

Detectable warning surfaces should not be provided at crossings of residential driveways, since the pedestrian right-of-way continues across residential driveway aprons. However, where commercial driveways are provided with yield or stop control, detectable warning surfaces should be provided at the junction between the pedestrian route and the vehicular route.

#### **41-6.09 Pedestrian Street Crossing**

All pedestrian street crossings shall be accessible to pedestrians with disabilities. Where pedestrian signals are provided at pedestrian street crossings, they should consider accessible pedestrian signals

#### **41-6.10 Protruding Objects**

Objects along or overhanging any portion of a pedestrian circulation path shall not reduce the clear width required for the PAR.

#### **41-6.11 Signs**

Where audible sign systems and other technologies are used to provide information equivalent to the information contained on pedestrian signs and transit signs, the signs are not required to comply with accessibility requirements.

Audible sign systems and other technologies that provide information equivalent to the information contained on signs are more usable by pedestrians who are blind or have low vision. Remote infrared audible signs that transmit information to portable devices that are carried by and audible only to the user are an example of audible sign systems and other technologies.

#### **41-6.12 Transit Stops and Transit Shelters**

Where provided (regardless of who provides), transit stops and transit shelters shall comply with accessibility requirements.

Transit stops in the public right-of-way typically serve fixed route bus systems, including bus rapid transit systems, and light rail transit systems. Signs that identify the routes served by the transit stop shall comply with the technical requirements for visual characters on signs unless audible sign systems or other technologies are used to provide the information. The FHWA has issued guidance on the obligations of state transportation departments, metropolitan planning organizations, and transit agencies to coordinate the planning and funding of accessibility improvements to transit systems and facilities.

**41-6.13 On-Street Parking**

**41-6.13(a) General**

Where on-street parking is provided on the block perimeter and the parking is marked or metered (including pay stations), accessible parking spaces shall be provided. Where parking pay stations are provided and the parking is not marked, each 20.0 ft (6.1 m) of block perimeter where parking is permitted shall be counted as one parking space. Figure 41-6C specifies how many accessible parking spaces shall be provided on the block perimeter where on-street parking is marked or metered.

Accessible parking spaces shall be identified by signs complying with the *ILMUTCD*. Accessible parking spaces should be located where the street has the least crown and grade and close to key destinations.

Total Number of Marked or Metered Parking Spaces on the Block Perimeter	Minimum Required Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 and over	4 percent of total

**ON-STREET PARKING SPACES**

**Figure 41-6C**

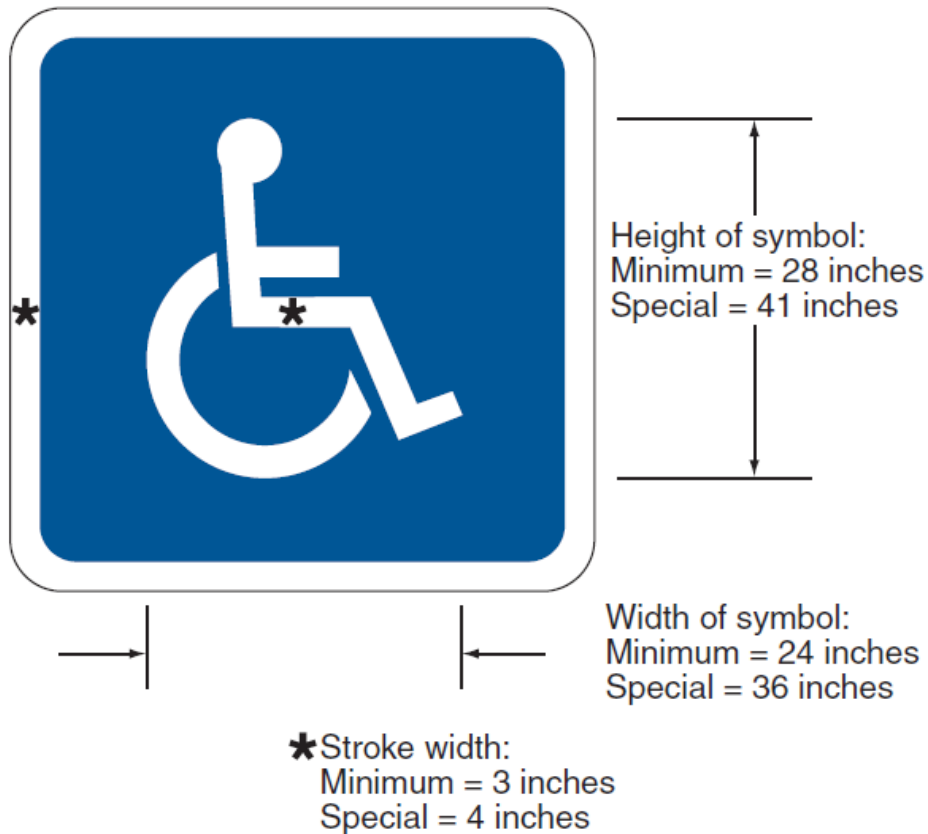
**41-6.13(b) Signage**

Accessible parking spaces shall be designated with a ground-mounted RESERVED PARKING for persons with disabilities (R7-8) sign with the \$\_\_\_\_ FINE (R7-I101) supplemental plaque. The amount of the fine shall be as established by 625 ILCS 5/11-1301.3(c). Signs shall be 7 ft (2.0 m) minimum above the finish floor or ground surface measured to the bottom of the sign, and no more than 2 ft (0.6 m) horizontally from the face of curb. Signs shall be located at the head or foot of the parking space so as not to interfere with the operation of a side lift or a passenger side transfer.

**41-6.13(c) Pavement Marking**

1. Color. Accessible parking spaces and access aisles shall be designated with white pavement marking and may be supplemented with blue pavement markings.
2. Width. Pavement marking shall be 3 in (75 mm) to 6 in (150 mm) wide.

3. Access Aisle. When required, the access aisle shall be marked with 45 degree diagonal lines. The diagonal lines should be 3 in (75 mm) to 6 in (150 mm) wide and separated by gaps of 12 in (300 mm) to 36 in (900 mm). The gap between the lines should not exceed 6 times the width of the diagonal lines.
4. Symbol. The international symbol of accessibility should be used to provide additional emphasis. See Figure 41-6D.



Note: Blue background and white border are optional

### INTERNATIONAL SYMBOL OF ACCESSIBILITY FOR PAVEMENT MARKING

Figure 41-6D

#### 41-6.13(d) Dimensions

Where parking spaces are marked with lines, width measurements of parking spaces and access aisles shall be made from the centerline of the markings. The required size of parking spaces and access aisles shall be determine by the type of parking space.



#### **41-6.13(e) Curb Ramps or Blended Transitions**

Curb ramps or blended transitions complying with Section 41-6.07 shall connect the access aisle to the PAR. Curb ramps shall not be located within the access aisle. At parallel parking spaces, curb ramps and blended transitions should be located so that a van side-lift or ramp can be deployed to the sidewalk and the vehicle occupant can transfer to a wheelchair or scooter. Parking spaces at the end of the block face may be served by curb ramps or blended transitions at the pedestrian street crossing, provided that the curb ramp is not accessed from a travel lane.

Detectable warning surfaces are not required on curb ramps and blended transitions that connect the access aisle to the sidewalk, including where the sidewalk is at the same level as the parking spaces, unless the curb ramps and blended transitions also serve pedestrian street crossings.

#### **41-6.14 Off-Street Parking**

##### **41-6.14(a) General**

Accessible parking spaces for individuals with disabilities and accessible passenger loading zones that serve a particular building shall be the spaces or zones closest to the nearest accessible entrance on a PAR. In separate parking facilities that do not serve a particular building, locate parking spaces for individuals with disabilities on the shortest possible circulation route to an accessible pedestrian entrance of the parking facility. In buildings with multiple access entrances with adjacent parking, accessible parking spaces may be dispersed and located closest to the accessible entrances. The total number of accessible parking spaces may be distributed among parking facilities, if greater accessibility is achieved in consideration of such factors as anticipated usage, number and location of entrances and level parking areas. Figure 41-6E specifies how many accessible parking spaces shall be provided.

Each parking space shall have its own access aisle and all access aisles shall blend to a common level with an accessible route. Parking spaces and access aisles shall be level with surface slopes not exceeding 1:50 (2.0%) in all directions. Minimum vertical clearance of 98 in (2490 mm) at the parking space and along at least one vehicle access route to such spaces from site entrance(s) and exit(s) shall be provided.

**BUREAU OF LOCAL ROADS & STREETS**  
**SPECIAL DESIGN ELEMENTS**

41-6(10)

Oct 2013

Total Number of Marked or Metered Parking Spaces on the Block Perimeter	Minimum Required Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2 percent of total
1001 and over	20, plus 1 for each 100, or fraction thereof, over 1000

**OFF-STREET PARKING SPACES**

**Figure 41-6E**

**41-6.14(b) Signage**

See Section 41-6.13(b).

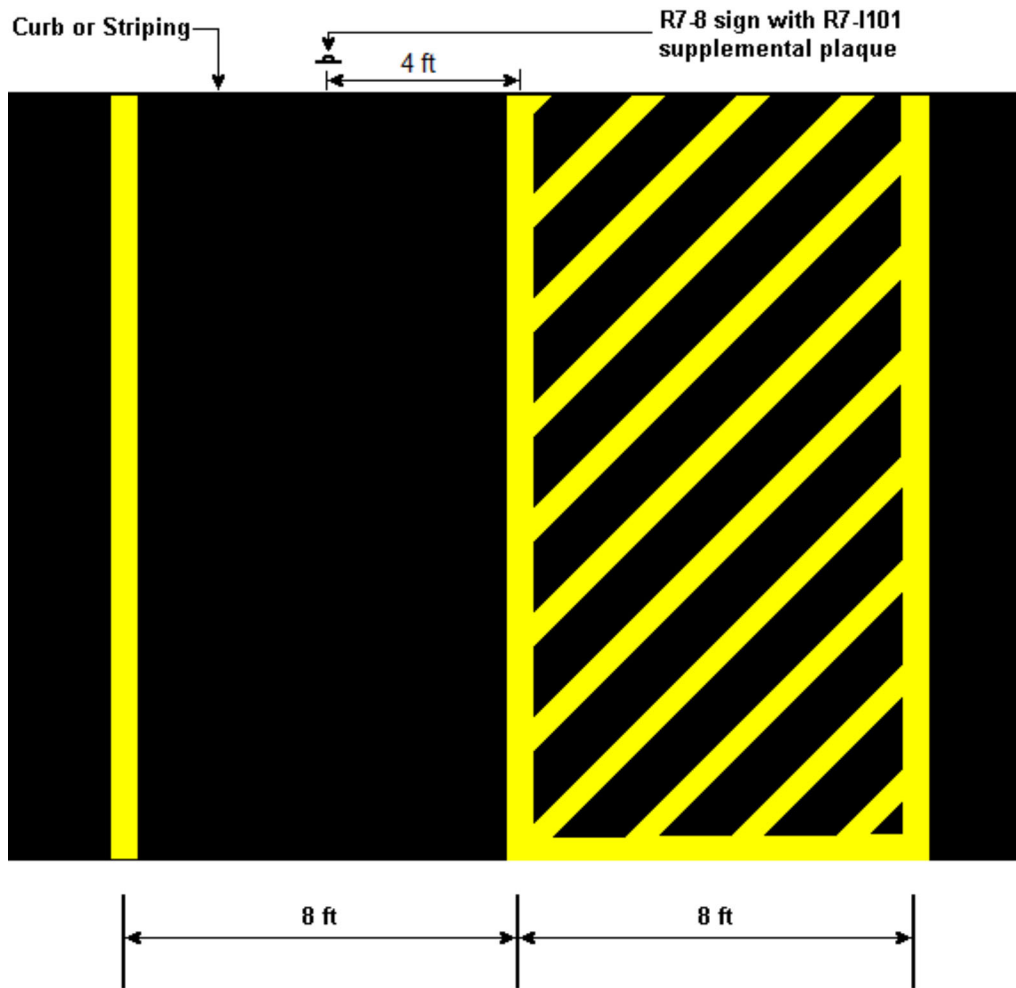
**41-6.14(c) Pavement Marking**

1. Color. Accessible parking spaces and access aisles shall be designated with yellow pavement marking.
2. Width. See Section 41-6.13(c).
3. Access Aisle. See Section 41-6.13(c).
4. Symbol. See Section 41-6.13(c).

**41-6.14(d) Dimensions**

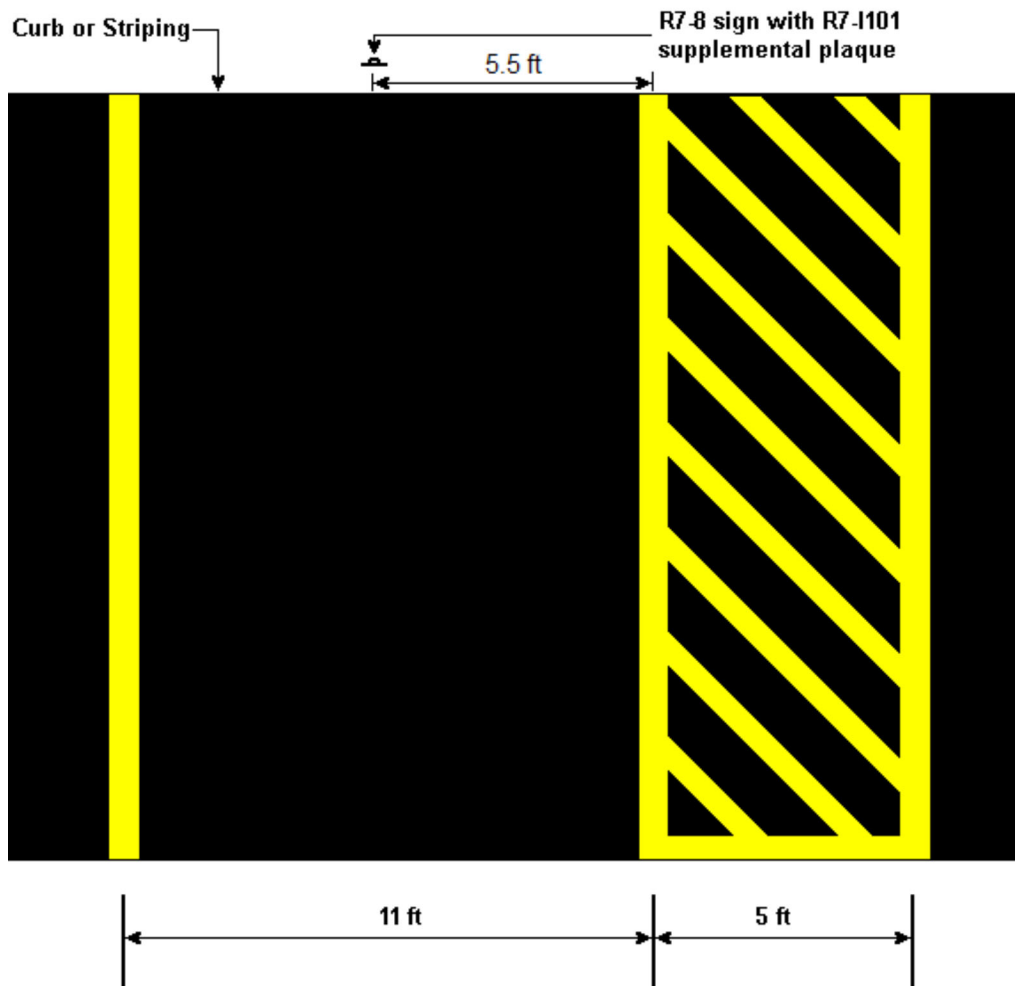
An accessible parking space shall consist of a 16 ft (4.9 m) wide parking space, which includes an 8 ft (2.4 m) wide access aisle (See Figure 41-6F). In the alternative, an accessible space may be 16 ft (4.9 m) wide, including a space 11 ft (3.4 m) wide with a 5 ft (1.5 m) access aisle (See Figure 41-6G).

A common access aisle shall not be shared between adjacent parking spaces. Access aisles shall be permitted to be placed on either side of the parking space except for angled parking spaces, which shall have access aisles located on the passenger side of the parking space.



**STANDARD OFF-STREET ACCESSIBLE PARKING SPACES**

**Figure 41-6F**



**UNIVERSAL PARKING DESIGN OFF-STREET ACCESSIBLE PARKING SPACES**

**Figure 41-6G**

**41-6.15 Passenger Loading Zones**

Where passenger loading zones other than transit stops are provided, at least one accessible passenger loading zone shall be provided for each 100.0 ft (30 m) of continuous loading zone space or fraction thereof. Accessible passenger loading zones must be identified by signs displaying the International Symbol of Accessibility.

**41-6.16 Other**

Where provided on pedestrian circulation paths, Stairways and Escalators (shall not be part of a PAR), Handrails, Doors, Doorways, and Gates; and Ramps shall comply with accessibility requirements.

# EXHIBIT F

## ANTICIPATED REPLACEMENT COSTS

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
1		X					\$ 2,175.00
2		X					\$ 2,175.00
3		X					\$ 2,175.00
4		X					\$ 2,175.00
5		X					\$ 2,175.00
6		X					\$ 2,175.00
7		X					\$ 2,175.00
9	X						\$ 2,675.00
10	X						\$ 2,675.00
11	X						\$ 2,675.00
12	X						\$ 2,675.00
13	X						\$ 2,675.00
14						X	\$ 530.00
17	X						\$ 2,675.00
18	X					X	\$ 530.00
19	X						\$ 2,675.00
20	X					X	\$ 530.00
21	X					X	\$ 530.00
22	X						\$ 2,675.00
23	X						\$ 2,675.00
24	X						\$ 2,675.00
25	X						\$ 2,675.00
26			X				\$ 5,820.00
29			X				\$ 5,820.00
30	X						\$ 2,675.00
31	X						\$ 2,675.00
32		X				X	\$ 530.00
33		X					\$ 2,175.00
34		X					\$ 2,175.00
35		X					\$ 2,175.00
36		X					\$ 2,175.00
37		X					\$ 2,175.00
38		X					\$ 2,175.00
39		X				X	\$ 530.00
40		X					\$ 2,175.00
41		X					\$ 2,175.00
42		X					\$ 2,175.00
44		X					\$ 2,175.00
45		X					\$ 2,175.00
46		X					\$ 2,175.00
47		X					\$ 2,175.00
48		X					\$ 2,175.00
49		X					\$ 2,175.00
50		X					\$ 2,175.00
52		X				X	\$ 530.00
53		X					\$ 2,175.00
54		X					\$ 2,175.00
55	X						\$ 2,675.00
56	X						\$ 2,675.00
57	X						\$ 2,675.00
58	X						\$ 2,675.00
59	X						\$ 2,675.00
60	X						\$ 2,675.00
61	X						\$ 2,675.00
62	X						\$ 2,675.00
63			X				\$ 5,820.00
64	X						\$ 2,675.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
65	X						\$ 2,675.00
66	X						\$ 2,675.00
67	X						\$ 2,675.00
68	X						\$ 2,675.00
69	X						\$ 2,675.00
70		X					\$ 2,175.00
71		X					\$ 2,175.00
72		X				X	\$ 530.00
73		X					\$ 2,175.00
74		X					\$ 2,175.00
75		X					\$ 2,175.00
76		X					\$ 2,175.00
77		X					\$ 2,175.00
78		X					\$ 2,175.00
79		X				X	\$ 530.00
80		X					\$ 2,175.00
81		X				X	\$ 530.00
82		X					\$ 2,175.00
83		X					\$ 2,175.00
84		X					\$ 2,175.00
85		X					\$ 2,175.00
86		X					\$ 2,175.00
87		X					\$ 2,175.00
88		X					\$ 2,175.00
89		X					\$ 2,175.00
90		X				X	\$ 530.00
91			X				\$ 5,820.00
92			X				\$ 5,820.00
93			X			X	\$ 530.00
94	X				X		\$ 2,675.00
95	X				X		\$ 2,675.00
96		X					\$ 2,175.00
97		X					\$ 2,175.00
98		X					\$ 2,175.00
99		X					\$ 2,175.00
100		X				X	\$ 530.00
101	X						\$ 2,675.00
102	X						\$ 2,675.00
103	X					X	\$ 530.00
104	X						\$ 2,675.00
105	X						\$ 2,675.00
106	X						\$ 2,675.00
107	X						\$ 2,675.00
108	X						\$ 2,675.00
109	X						\$ 2,675.00
110	X						\$ 2,675.00
111		X					\$ 2,175.00
112		X					\$ 2,175.00
113		X					\$ 2,175.00
114		X					\$ 2,175.00
115		X					\$ 2,175.00
116		X					\$ 2,175.00
117		X					\$ 2,175.00
118		X					\$ 2,175.00
119		X					\$ 2,175.00
120		X					\$ 2,175.00
121		X				X	\$ 530.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
122	X						\$ 2,675.00
123	X						\$ 2,675.00
124	X						\$ 2,675.00
125	X						\$ 2,675.00
126		X					\$ 2,175.00
127				X			\$ 4,575.00
128		X					\$ 2,175.00
129		X					\$ 2,175.00
131		X					\$ 2,175.00
132		X					\$ 2,175.00
134	X						\$ 2,675.00
135	X						\$ 2,675.00
136	X						\$ 2,675.00
137	X					X	\$ 530.00
138	X						\$ 2,675.00
139	X						\$ 2,675.00
140	X					X	\$ 530.00
141	X						\$ 2,675.00
142	X						\$ 2,675.00
143	X						\$ 2,675.00
144	X						\$ 2,675.00
145	X					X	\$ 530.00
146	X						\$ 2,675.00
147	X						\$ 2,675.00
148	X						\$ 2,675.00
149	X						\$ 2,675.00
150	X						\$ 2,675.00
151	X						\$ 2,675.00
152	X						\$ 2,675.00
153	X						\$ 2,675.00
154	X					X	\$ 530.00
155	X						\$ 2,675.00
156	X						\$ 2,675.00
157	X					X	\$ 530.00
160	X						\$ 2,675.00
161	X						\$ 2,675.00
162	X						\$ 2,675.00
163	X						\$ 2,675.00
164	X					X	\$ 530.00
165	X						\$ 2,675.00
166	X						\$ 2,675.00
167	X					X	\$ 530.00
168	X					X	\$ 530.00
169	X						\$ 2,675.00
170	X						\$ 2,675.00
171	X					X	\$ 530.00
172	X						\$ 2,675.00
173	X						\$ 2,675.00
174	X						\$ 2,675.00
175	X						\$ 2,675.00
176	X						\$ 2,675.00
177	X						\$ 2,675.00
178	X						\$ 2,675.00
179	X						\$ 2,675.00
180	X						\$ 2,675.00
181	X						\$ 2,675.00
182	X						\$ 2,675.00



**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
183	X						\$ 2,675.00
184	X						\$ 2,675.00
185	X						\$ 2,675.00
186	X						\$ 2,675.00
187	X						\$ 2,675.00
188	X					X	\$ 530.00
189	X						\$ 2,675.00
190	X						\$ 2,675.00
191	X					X	\$ 530.00
192	X						\$ 2,675.00
193	X						\$ 2,675.00
194	X						\$ 2,675.00
195	X						\$ 2,675.00
196	X						\$ 2,675.00
197	X						\$ 2,675.00
198	X						\$ 2,675.00
199	X						\$ 2,675.00
201						X	\$ 530.00
202	X						\$ 2,675.00
203	X					X	\$ 530.00
204	X					X	\$ 530.00
205	X						\$ 2,675.00
206	X						\$ 2,675.00
207				X		X	\$ 530.00
208				X		X	\$ 530.00
211		X				X	\$ 530.00
212		X					\$ 2,175.00
213		X					\$ 2,175.00
214	X						\$ 2,675.00
215	X						\$ 2,675.00
216	X						\$ 2,675.00
217			X			X	\$ 530.00
221	X						\$ 2,675.00
222	X						\$ 2,675.00
223	X						\$ 2,675.00
224	X						\$ 2,675.00
225	X					X	\$ 530.00
226	X						\$ 2,675.00
227	X						\$ 2,675.00
228	X						\$ 2,675.00
229	X						\$ 2,675.00
230	X						\$ 2,675.00
231	X						\$ 2,675.00
232	X						\$ 2,675.00
233	X						\$ 2,675.00
234	X						\$ 2,675.00
235	X						\$ 2,675.00
236	X						\$ 2,675.00
237	X						\$ 2,675.00
238	X						\$ 2,675.00
239	X						\$ 2,675.00
240	X						\$ 2,675.00
241	X						\$ 2,675.00
242	X						\$ 2,675.00
243	X						\$ 2,675.00
244	X					X	\$ 530.00
245	X					X	\$ 530.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
246	X						\$ 2,675.00
247	X						\$ 2,675.00
248	X					X	\$ 530.00
249	X					X	\$ 530.00
250	X						\$ 2,675.00
251	X						\$ 2,675.00
253	X						\$ 2,675.00
254	X					X	\$ 530.00
255	X						\$ 2,675.00
256	X						\$ 2,675.00
258	X						\$ 2,675.00
259	X						\$ 2,675.00
264			X				\$ 5,820.00
265	X						\$ 2,675.00
266	X						\$ 2,675.00
267	X						\$ 2,675.00
271	X						\$ 2,675.00
272	X						\$ 2,675.00
273	X						\$ 2,675.00
275	X						\$ 2,675.00
276	X						\$ 2,675.00
277	X						\$ 2,675.00
278	X						\$ 2,675.00
281	X						\$ 2,675.00
282	X					X	\$ 530.00
283	X						\$ 2,675.00
284	X						\$ 2,675.00
285	X						\$ 2,675.00
286	X						\$ 2,675.00
287	X						\$ 2,675.00
288	X						\$ 2,675.00
289	X						\$ 2,675.00
290	X					X	\$ 530.00
291	X						\$ 2,675.00
292	X						\$ 2,675.00
293	X						\$ 2,675.00
295	X						\$ 2,675.00
296		X				X	\$ 530.00
297	X						\$ 2,675.00
298	X						\$ 2,675.00
299	X						\$ 2,675.00
300	X						\$ 2,675.00
301	X						\$ 2,675.00
302	X						\$ 2,675.00
303	X						\$ 2,675.00
304	X						\$ 2,675.00
305	X						\$ 2,675.00
306	X						\$ 2,675.00
307	X						\$ 2,675.00
308	X						\$ 2,675.00
309	X						\$ 2,675.00
310	X						\$ 2,675.00
311	X						\$ 2,675.00
312	X						\$ 2,675.00
313	X						\$ 2,675.00
314	X						\$ 2,675.00
315	X						\$ 2,675.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
316	X						\$ 2,675.00
317	X						\$ 2,675.00
318	X						\$ 2,675.00
319	X						\$ 2,675.00
320	X						\$ 2,675.00
321	X						\$ 2,675.00
322	X						\$ 2,675.00
323	X						\$ 2,675.00
324	X					X	\$ 530.00
325	X						\$ 2,675.00
326	X						\$ 2,675.00
327	X						\$ 2,675.00
328		X				X	\$ 530.00
331	X						\$ 2,675.00
332	X						\$ 2,675.00
333		X					\$ 2,175.00
334		X					\$ 2,175.00
335		X					\$ 2,175.00
336		X					\$ 2,175.00
337		X					\$ 2,175.00
338		X					\$ 2,175.00
339	X						\$ 2,675.00
340	X						\$ 2,675.00
342	X						\$ 2,675.00
343	X						\$ 2,675.00
344	X						\$ 2,675.00
345	X						\$ 2,675.00
346		X					\$ 2,175.00
347				X			\$ 4,575.00
348		X				X	\$ 530.00
349		X				X	\$ 530.00
350		X				X	\$ 530.00
351		X				X	\$ 530.00
352		X					\$ 2,175.00
353		X					\$ 2,175.00
354		X					\$ 2,175.00
355		X					\$ 2,175.00
356		X					\$ 2,175.00
357		X					\$ 2,175.00
358		X					\$ 2,175.00
359		X					\$ 2,175.00
360		X					\$ 2,175.00
361		X					\$ 2,175.00
362		X					\$ 2,175.00
363		X					\$ 2,175.00
364		X					\$ 2,175.00
365		X					\$ 2,175.00
366		X					\$ 2,175.00
369		X					\$ 2,175.00
371		X				X	\$ 530.00
372		X					\$ 2,175.00
373		X					\$ 2,175.00
374		X					\$ 2,175.00
375		X					\$ 2,175.00
376		X					\$ 2,175.00
377		X					\$ 2,175.00
378		X					\$ 2,175.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
379		X					\$ 2,175.00
392		X					\$ 2,175.00
393		X					\$ 2,175.00
394		X					\$ 2,175.00
395		X				X	\$ 530.00
396		X					\$ 2,175.00
397		X					\$ 2,175.00
398		X					\$ 2,175.00
399		X					\$ 2,175.00
400		X					\$ 2,175.00
401		X					\$ 2,175.00
402		X					\$ 2,175.00
403		X					\$ 2,175.00
404		X					\$ 2,175.00
405		X					\$ 2,175.00
406		X					\$ 2,175.00
407		X					\$ 2,175.00
408		X					\$ 2,175.00
409		X					\$ 2,175.00
410		X					\$ 2,175.00
411		X					\$ 2,175.00
412		X					\$ 2,175.00
413		X					\$ 2,175.00
414		X					\$ 2,175.00
415		X					\$ 2,175.00
416		X					\$ 2,175.00
425		X					\$ 2,175.00
426		X					\$ 2,175.00
427		X					\$ 2,175.00
428		X					\$ 2,175.00
429		X					\$ 2,175.00
430		X					\$ 2,175.00
435		X					\$ 2,175.00
436		X					\$ 2,175.00
437			X				\$ 5,820.00
438	X						\$ 2,675.00
439	X						\$ 2,675.00
440	X						\$ 2,675.00
441	X						\$ 2,675.00
442	X						\$ 2,675.00
443	X						\$ 2,675.00
444	X						\$ 2,675.00
445	X						\$ 2,675.00
446	X						\$ 2,675.00
447	X						\$ 2,675.00
448	X						\$ 2,675.00
449		X					\$ 2,175.00
450		X				X	\$ 530.00
451		X					\$ 2,175.00
452			X				\$ 5,820.00
454			X				\$ 5,820.00
455			X				\$ 5,820.00
456			X				\$ 5,820.00
457			X				\$ 5,820.00
458	X						\$ 2,675.00
459		X					\$ 2,175.00
460	X						\$ 2,675.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
462	X						\$ 2,675.00
463	X						\$ 2,675.00
466		X					\$ 2,175.00
467		X					\$ 2,175.00
468		X					\$ 2,175.00
469		X					\$ 2,175.00
470		X					\$ 2,175.00
471		X				X	\$ 530.00
472	X						\$ 2,675.00
473	X						\$ 2,675.00
474		X					\$ 2,175.00
475		X					\$ 2,175.00
476		X				X	\$ 530.00
477		X					\$ 2,175.00
478		X					\$ 2,175.00
479		X					\$ 2,175.00
480			X				\$ 5,820.00
483		X					\$ 2,175.00
484		X					\$ 2,175.00
493		X				X	\$ 530.00
498		X					\$ 2,175.00
499		X					\$ 2,175.00
500		X					\$ 2,175.00
501		X					\$ 2,175.00
502		X					\$ 2,175.00
503		X					\$ 2,175.00
504	X						\$ 2,675.00
505		X					\$ 2,175.00
506	X						\$ 2,675.00
507	X						\$ 2,675.00
508	X						\$ 2,675.00
513	X						\$ 2,675.00
514	X						\$ 2,675.00
516	X						\$ 2,675.00
520	X						\$ 2,675.00
522	X						\$ 2,675.00
523	X						\$ 2,675.00
524	X						\$ 2,675.00
525	X						\$ 2,675.00
527	X						\$ 2,675.00
528	X						\$ 2,675.00
529			X				\$ 5,820.00
530			X				\$ 5,820.00
537	X						\$ 2,675.00
538	X						\$ 2,675.00
539		X				X	\$ 530.00
540		X				X	\$ 530.00
541		X					\$ 2,175.00
542		X					\$ 2,175.00
543		X					\$ 2,175.00
544		X					\$ 2,175.00
545		X					\$ 2,175.00
546		X					\$ 2,175.00
547		X					\$ 2,175.00
548		X					\$ 2,175.00
549		X				X	\$ 530.00
550		X				X	\$ 530.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
551		X					\$ 2,175.00
552		X					\$ 2,175.00
553		X					\$ 2,175.00
554		X					\$ 2,175.00
555		X					\$ 2,175.00
556		X					\$ 2,175.00
557		X					\$ 2,175.00
558		X					\$ 2,175.00
559	X						\$ 2,675.00
560	X						\$ 2,675.00
561	X						\$ 2,675.00
562	X						\$ 2,675.00
563	X						\$ 2,675.00
564	X						\$ 2,675.00
565	X						\$ 2,675.00
566	X						\$ 2,675.00
569		X					\$ 2,175.00
571		X					\$ 2,175.00
572	X						\$ 2,675.00
573		X					\$ 2,175.00
574		X				X	\$ 530.00
575		X				X	\$ 530.00
576		X					\$ 2,175.00
577		X					\$ 2,175.00
578		X					\$ 2,175.00
579		X					\$ 2,175.00
580		X					\$ 2,175.00
581		X				X	\$ 530.00
582		X					\$ 2,175.00
583		X					\$ 2,175.00
584		X					\$ 2,175.00
585		X					\$ 2,175.00
586		X					\$ 2,175.00
587		X					\$ 2,175.00
588		X					\$ 2,175.00
589		X					\$ 2,175.00
590		X					\$ 2,175.00
591		X					\$ 2,175.00
592		X					\$ 2,175.00
593		X					\$ 2,175.00
594	X						\$ 2,675.00
595	X						\$ 2,675.00
596	X						\$ 2,675.00
597	X						\$ 2,675.00
598	X						\$ 2,675.00
599	X						\$ 2,675.00
600	X						\$ 2,675.00
601	X						\$ 2,675.00
605		X					\$ 2,175.00
606		X					\$ 2,175.00
607		X					\$ 2,175.00
608		X					\$ 2,175.00
609		X					\$ 2,175.00
610		X				X	\$ 530.00
611	X						\$ 2,675.00
612	X						\$ 2,675.00
613	X						\$ 2,675.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
614	X						\$ 2,675.00
615	X						\$ 2,675.00
616	X						\$ 2,675.00
620	X						\$ 2,675.00
621	X						\$ 2,675.00
622	X						\$ 2,675.00
623		X					\$ 2,175.00
624		X					\$ 2,175.00
625	X						\$ 2,675.00
626	X						\$ 2,675.00
627	X						\$ 2,675.00
628	X						\$ 2,675.00
629	X						\$ 2,675.00
630	X						\$ 2,675.00
631		X					\$ 2,175.00
632		X					\$ 2,175.00
633	X						\$ 2,675.00
634	X						\$ 2,675.00
635	X						\$ 2,675.00
636	X						\$ 2,675.00
640	X						\$ 2,675.00
643	X						\$ 2,675.00
644	X						\$ 2,675.00
645	X						\$ 2,675.00
646	X					X	\$ 530.00
647	X						\$ 2,675.00
648	X						\$ 2,675.00
649	X						\$ 2,675.00
650	X						\$ 2,675.00
651	X						\$ 2,675.00
652	X						\$ 2,675.00
653	X						\$ 2,675.00
656	X						\$ 2,675.00
657	X					X	\$ 530.00
660	X					X	\$ 530.00
661	X					X	\$ 530.00
664	X						\$ 2,675.00
665	X						\$ 2,675.00
666	X						\$ 2,675.00
667	X						\$ 2,675.00
668		X					\$ 2,175.00
669		X				X	\$ 530.00
670	X						\$ 2,675.00
671	X						\$ 2,675.00
672	X						\$ 2,675.00
673	X						\$ 2,675.00
674	X						\$ 2,675.00
675	X					X	\$ 530.00
676	X						\$ 2,675.00
677	X						\$ 2,675.00
678	X						\$ 2,675.00
679	X						\$ 2,675.00
680	X					X	\$ 530.00
681	X						\$ 2,675.00
682	X					X	\$ 530.00
683	X					X	\$ 530.00
684	X						\$ 2,675.00

**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
686	X					X	\$ 530.00
692	X						\$ 2,675.00
693			X				\$ 5,820.00
694		X					\$ 2,175.00
695	X						\$ 2,675.00
696	X						\$ 2,675.00
697	X						\$ 2,675.00
698	X						\$ 2,675.00
699		X					\$ 2,175.00
700		X					\$ 2,175.00
701		X					\$ 2,175.00
702			X				\$ 5,820.00
703	X						\$ 2,675.00
704	X						\$ 2,675.00
705	X						\$ 2,675.00
706			X				\$ 5,820.00
707	X						\$ 2,675.00
708		X					\$ 2,175.00
709		X					\$ 2,175.00
710		X					\$ 2,175.00
711	X						\$ 2,675.00
712	X						\$ 2,675.00
713	X						\$ 2,675.00
714	X						\$ 2,675.00
715	X						\$ 2,675.00
716	X						\$ 2,675.00
717	X						\$ 2,675.00
718	X						\$ 2,675.00
719		X					\$ 2,175.00
720		X					\$ 2,175.00
721		X					\$ 2,175.00
722		X					\$ 2,175.00
723	X						\$ 2,675.00
724	X						\$ 2,675.00
725	X						\$ 2,675.00
726	X						\$ 2,675.00
727	X						\$ 2,675.00
728	X						\$ 2,675.00
729	X						\$ 2,675.00
732	X					X	\$ 530.00
733	X					X	\$ 530.00
734	X						\$ 2,675.00
735	X						\$ 2,675.00
736	X						\$ 2,675.00
737	X						\$ 2,675.00
738	X						\$ 2,675.00
740	X					X	\$ 530.00
742	X					X	\$ 530.00
743		X					\$ 2,175.00
744		X					\$ 2,175.00
745		X					\$ 2,175.00
746		X					\$ 2,175.00
747	X						\$ 2,675.00
748	X						\$ 2,675.00
749	X					X	\$ 530.00
750	X						\$ 2,675.00
751		X					\$ 2,175.00



**EXHIBIT F - ANTICIPATED REPLACEMENT COSTS**

RAMP ID #	TYPE OF RAMP						Estimated Cost for Non-Compliant Ramp Replacements
	Curb Ramp	Curbless Ramp	Depressed Corner	Curbless Depressed Corner	Bike Path	Ramp to Nowhere	
752	X						\$ 2,675.00
753		X				X	\$ 530.00
754		X				X	\$ 530.00
755		X					\$ 2,175.00
756	X						\$ 2,675.00
757		X					\$ 2,175.00
758		X					\$ 2,175.00
759		X				X	\$ 530.00
760		X					\$ 2,175.00
761		X					\$ 2,175.00
762		X					\$ 2,175.00
764		X				X	\$ 530.00
765		X					\$ 2,175.00
766		X				X	\$ 530.00
767		X					\$ 2,175.00
768		X					\$ 2,175.00
769	X						\$ 2,675.00
771		X					\$ 2,175.00
772		X					\$ 2,175.00
773		X					\$ 2,175.00
774		X					\$ 2,175.00
775		X				X	\$ 530.00
776	X						\$ 2,675.00
777	X						\$ 2,675.00
778	X					X	\$ 530.00
779		X					\$ 2,175.00
780		X					\$ 2,175.00
781		X					\$ 2,175.00
782		X				X	\$ 530.00
783		X					\$ 2,175.00
784		X					\$ 2,175.00
<b>TOTAL</b>	429	325	23	5	2	85	\$ 1,524,235.00